

REVISED PUBLIC HEARING DRAFT (11.9.00)



# Claremont Village Expansion Area Specific Plan

Prepared For:



City of Claremont  
Claremont Redevelopment Agency  
207 Harvard Avenue  
Claremont, California 91711-0880

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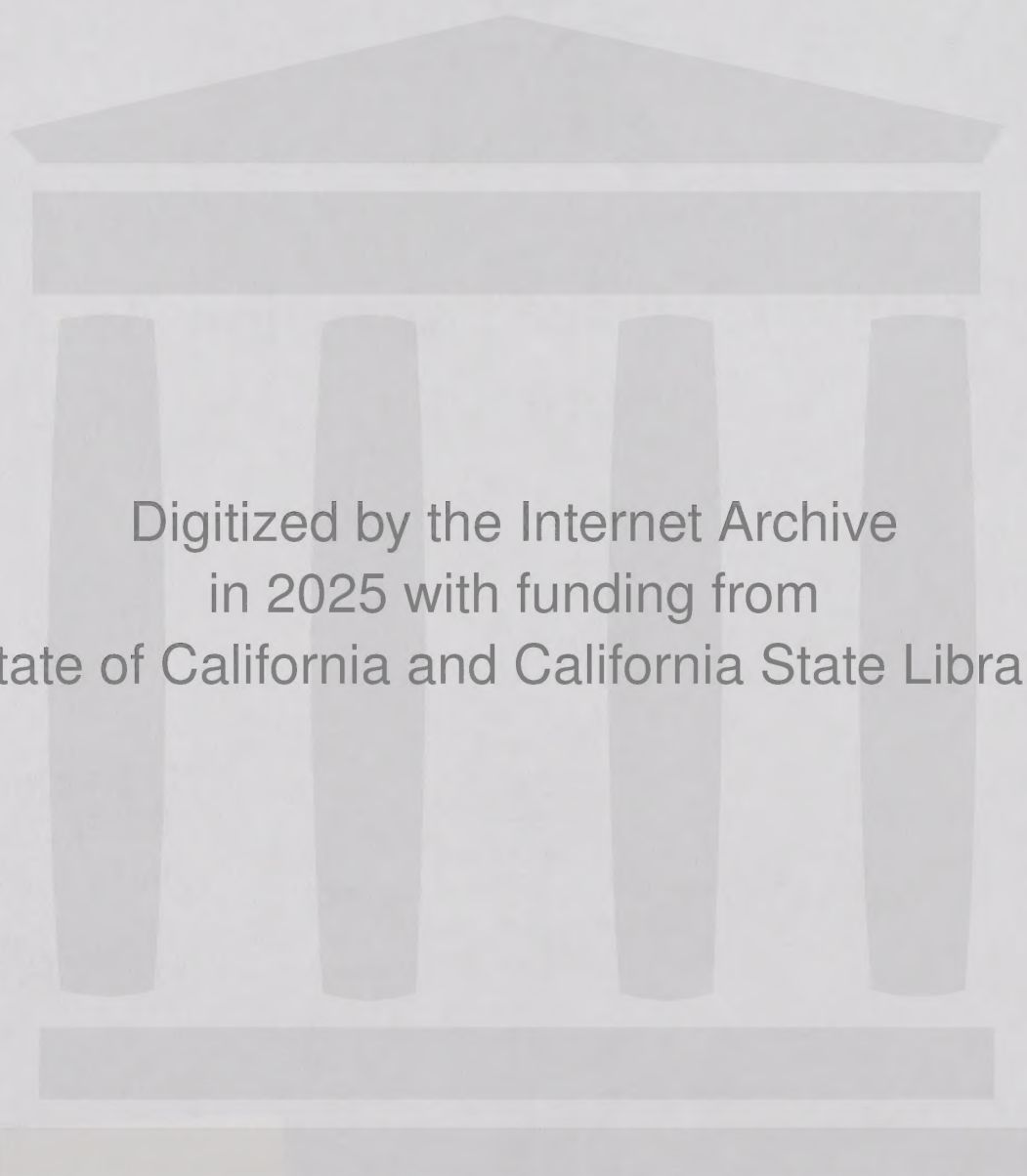
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# Claremont Village Expansion Area Specific Plan Public Hearing Draft

Resolution Number \_\_\_\_\_  
November 9, 2000

**Consultants:**

**RRM Design Group  
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# 1.0 INTRODUCTION

## TOPICS

- 1.0. Introduction
- 1.1 Plan Synopsis
- 1.2 Purpose and Content
- 1.3 Plan Location and Setting
- 1.4 Community Vision
- 1.5 Public Participation Process and Data Gathering
- 1.6 Aerial Photographs and Base Mapping
- 1.7 Organization of the Specific Plan
  - Introduction Section
  - Land Use Section
  - Development Standards and Design Guidelines
  - Circulation Section
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  - Implementation Section
- 1.8 Relationship to the General Plan
- 1.9 Relationship to the Land Use and Development Code
- 1.10 The Design Review Process
- 1.11 Environmental Review/CEQA Exemption
- 1.12 Project Consistency
- 1.13 Severability Clause



## 1.1 Plan Synopsis

This Specific Plan sets forth the planning principles, land use policies, development standards, design guidelines and phasing for growth within the Claremont Village Expansion Area. The Specific Plan follows those requirements and policies identified in the Claremont General Plan, Land Use Plan, and the State of California requirements for specific plans (Section 65450 et. seq.). It describes the types of uses and character desired in the Village Expansion Area and the necessary infrastructure, public and private facilities, services, and private property development standards and design guidelines needed to accommodate new and infill development. Section 1.7 of this Specific Plan explains the organization of this Specific Plan and summarizes its contents.

## 1.2 Purpose and Content

The purpose of this Specific Plan is to establish a bridge between the City's General Plan and the development of individual properties within the Village Expansion Area. The overall intent is to provide for the orderly development and redevelopment of the Village Expansion Area in a method that is consistent with the General Plan and traditional development in the existing Village. This Specific Plan establishes land use and development standards that will govern development within the planning area.

This Specific Plan is a valuable tool because it explains and illustrates the development form that may be expected in the future. It sets forth a framework that will guide development of the Village as it expands based upon economic, environmental, and social needs of Claremont residents and visitors.

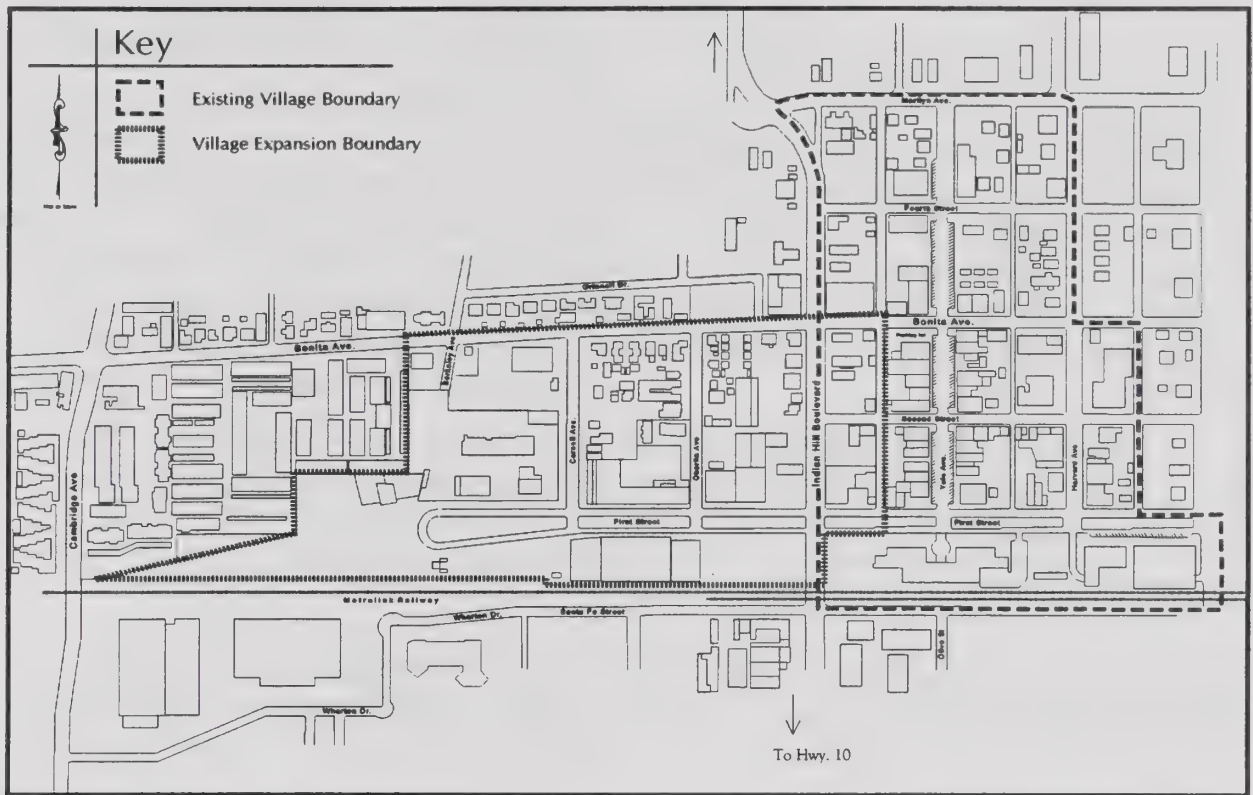
## 1.3 Plan Location and Setting

The Claremont Village Expansion Area comprises approximately 35 acres within the City limits of Claremont. The site is conveniently situated between Interstate 210/Foothill Boulevard and Interstate 10 and is adjacent to the City's downtown area. Specifically, the Specific Plan boundary includes one-half block of the existing Village east of Indian Hill Boulevard, stretching north to Bonita Avenue, west to Cambridge Avenue and the Claremont Village Green housing complex, and south to the Metrolink tracks as shown in Figure 1.

Claremont's Village was, and is, more than just bricks and mortar, more than just another shopping hub. It is the traditional center of a community, a center not defined so much geographically or architecturally as it is socially. It is a place where people doing a quick errand run into people they know and end up spending several hours together. It is a place for strolling and window shopping, and watching people go by. It is a place where merchants know customers by face if not by name. It is, in short, a place for activities of all kinds, for all kinds of people.







Vicinity Map

Figure 1

The physical design of the Village, however, is the backdrop or stageset to the street, viewing the drama on it. Although façades change, the traditional strength of commercial storefronts is their cohesion. Standing adjacent to each other, the commercial storefronts form the rhythm of the retail commercial area of the street. What makes Claremont rare within the suburban San Gabriel Valley is its thriving Village. Perpetuating the continued evolution of this cohesive urban fabric is the purpose of the Village Expansion Area Specific Plan.

The urban design patterns and physical amenities in the existing Village are like those in many towns that used to exist in many parts of this country before the advent of the regional shopping centers. Although land uses may have changed over the years, the downtown is still a vital area, showing distinctive eras of growth, architecture, and function. The eclectic architecture ranging from small clapboard structures which were once residences and bungalows to a few art deco, masonry buildings of the thirties, to modern wood and concrete structures, is scaled to pedestrians rather than to motor vehicles. The additions of colorful planters, comfortable street furniture, pedestrian scale lighting, and outdoor dining establishments have added to the ambience of the downtown. The Village is an organism, which tells a story of the town's growth.





## 1.4 Community Vision

This Specific Plan furthers the vision for the continued evolution of the existing Village and its Expansion Area, and implements development standards, design guidelines and allowable uses. Most importantly, it has been determined that the community as a whole recognizes the potential in continued investment in shaping and maintaining the Village environment westward into the Village Expansion Area, enriching the existing Village to create an attractive and thriving place for the enjoyment and benefit of everyone. Based upon numerous outreach efforts with City residents and businesses, as well as many field studies, the following vision statement has been recognized by the community:

*“To expand the Village to the west side of Indian Hill Boulevard by adding new retail, commercial, and housing opportunities while, at the same time, preserving the economic viability of the Village; to use the character, uniqueness, pedestrian orientation, and historic resources of the Village as the inspiration for the Village Expansion Area; and to provide for uses and amenities that add to, and cannot be accommodated in, the existing Village.”*

The vision for the Village Expansion Area is based on Claremont’s market niche, the City’s economic health, and the principles of new urbanism. This Specific Plan is guided by the following goals:

### The Goals

1. To provide effective guidelines and standards for the location, amount, type, and quality of new and expanded development in the Village Expansion Area.
2. To expand business and residential opportunities in the City and provide amenities to attract shoppers, visitors, students, and residents.
3. To enhance the economic base of the City.
4. To be sensitive to and mindful of historic roots of the City.
5. To develop additional public plazas, open spaces, and landscape/water features to serve as points of interest for the community.
6. To plan for appropriate land uses in the Village Expansion Area that take advantage of the proximity to the transit facilities, including the Depot and Metrolink station.



## 1.5 Public Participation Process and Data Gathering

The Claremont Village Expansion Area Specific Plan evolved over a two-year process and involved public outreach through a series of methods. Project consultants worked with the community and City leaders via a series of activities to solicit interaction and input so that the goals of the Village are indeed representative of the merchants, landowners, and public that regularly use the Village. Preferences were openly discussed with the City and its consultants, resulting in issues that should be addressed in the Specific Plan. Some of the community outreach and data gathering methods used in the Specific Plan process included:

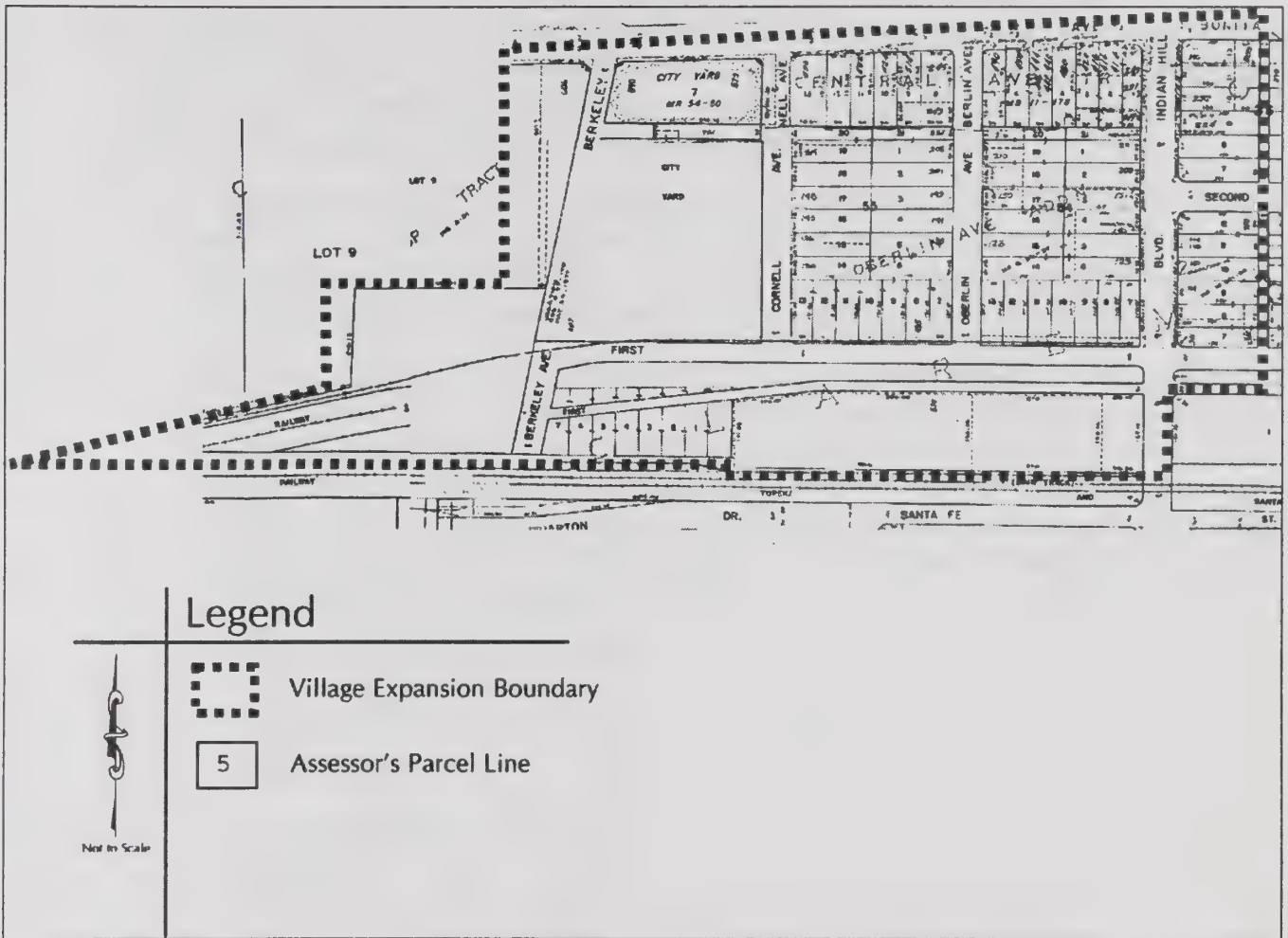
- Evaluation and mapping of Existing Conditions, Opportunities & Constraints
- Key Agency/Interest Groups/Person Interviews & Dialogue
- Character Imaging ~ Camera Survey of Community Likes and Dislikes
- Public Workshops/Design Charrette
- Traffic & Parking Analysis
- Formulation of Planning Principles
- Land Use and Circulation Options
- Newsletters, Press Coverage, and Open Houses
- Coordination with Private Commercial and Residential Development Interests on Public/Private Ventures on City Owned Properties
- Fiscal and Financial Studies Related to Development of City Property
- Planning/Architectural/Traffic & Transportation Commissions/City Council Meetings

## 1.6 Aerial Photographs and Base Mapping

In order to clearly articulate the vision for the Village Expansion Area and its relationship to the Village, an aerial photograph was referenced (together with assessor's parcel maps and available legal instruments of record) into a base map encompassing all the parcels in the Village Expansion Area. Existing Village parcel configurations are illustrated in Figure 2.







Existing Parcel Condition

Figure 2

## 1.7 Organization of the Specific Plan

The Specific Plan is an implementation tool for the Claremont General Plan, providing direction for development of the Village Expansion Area. When applicable, the Specific Plan will reference existing policies, guidelines, codes and documents already in use by the City on a daily basis. This cross-reference will reduce the need to continually update the Specific Plan document if changes to other regulatory documents occur over time. The Specific Plan will provide the user with information needed to guide development for a parcel within the Village Expansion Area. Future development proposals must be found consistent with this Specific Plan in order to obtain proper approvals and permitting. Where the Specific Plan refines the requirements of the Claremont General Plan, the Specific Plan provisions will take precedence.



The Specific Plan is divided into five (5) sections. The five sections include the Introduction, Village Expansion Land Uses, Development Standards and Design Guidelines, Circulation, Public Utilities, Infrastructure and Services, Implementation Plan, and Plan Administration, Adoption and Amendment. These sections are briefly described as follows:

- 1.0 Introduction Section – The Introduction to the Specific Plan addresses the overall intent of the Plan, summarizes the Plan’s primary components, describes the relationship of the Specific Plan to other City policy documents, and identifies the standing city goals that sponsored and guided formulation of the Specific Plan.
- 2.0 Land Use Section – The Land Use section locates and describes the different types of uses proposed in the planning area. The Village Expansion Area is divided into five land-use types with special reuse guidelines for select historic structures. These land use types correspond to existing development, new development, and reflect the creation of traditional commercial and residential neighborhood districts that set forth varying development potential for the Village Expansion Area. Planned land uses are divided into the following five categories:
  - Commercial Mixed-Use ( CMX )
  - Residential Mixed-Use ( RMX )
  - Mixed-Use ( MX )
  - Residential ( R )
  - Public (P)

In addition to the five primary land uses, some special criteria are set forth to guide historic site development of the Packing House and Ice House Office properties. These reuse criteria provide additional guidelines to ensure that careful consideration is given to reusing these historic properties.

This section also identifies the permitted uses within each zone and includes a development standards matrix that addresses the regulating provisions for new or replacement structures. A land use list is also included in this section that mandates permitted and conditionally permitted uses.

Section 2.4 *Specific Plan Development Standards and Design Guidelines* is a substantial subsection of the Land Use Section. It describes the development standards and design guidelines for each land use category. These development standards and design guidelines should be used during the preparation and review of all development and permitting applications. The design guidelines are prepared to provide direction on how to enhance and extend the visual charm and maintain design continuity and individuality in the Village Expansion Area. The development standards and design guidelines specifically address issues relating to architectural style, character façade improvements, landscaping, building renovation, and parking lot design.



- 3.0 Circulation Section – This section describes the planned street network hierarchy and their functions. It includes automobile, alternative transportation, and pedestrian circulation patterns. The Circulation Section discusses all forms of transportation needed to meet the needs of the Village Expansion Area, including the following:
- Entries to the Village Expansion Area
  - Primary and Secondary Streets
  - Planned Roadway Improvements
  - Public Streetscape Beautification
  - Public Transit
  - Pedestrian, Bicycle and Disabled Access
  - Parking Supply and Management
- 4.0 Public Utilities, Infrastructure and Services Section – This section describes planning area sewer, water and drainage infrastructure demand, supply and service, as well as the public services that will be affected by the Village Expansion Area development (such as solid waste and recycling, fire and police protection, schools, the City Corporation Yard, and public utilities).
- 5.0 Implementation Section – The Implementation Plan sets forth the implementation strategy for development of the Village Expansion Area. It identifies recommended phasing scenarios and planned improvement projects for implementation of this Plan.

## 1.8 Relationship to the General Plan

This Specific Plan is authorized by Article 8 of the California Government Code, Section 65450 et. set. This document meets the requirements as specified in Section 65451 of that Code. The Specific Plan is consistent with and furthers the objectives of the City of Claremont's General Plan by providing detailed criteria for development of specific sites and public streetscape improvements. This Specific Plan has been prepared to reinforce all elements of the General Plan adopted by the City Council on October 27, 1981 (including revisions to same since the original adoption). The following General Plan policies have guided the Village Expansion Area Specific Plan effort.





## Policies from Claremont General Plan for the Village Expansion Area:

### Land Use Element

1. The City shall encourage a variety of housing types to meet the personal and economic needs of the citizens of the community.
2. The City shall require Residential Unit Development or Specific Plan proposals on those properties that are found to be appropriate due to their size, relation to or impact on surrounding uses and the circulation system, or proximity to natural hazard areas.
3. The City shall promote neighborhood conservation and identity.
4. The City shall conserve the existing housing stock and encourage maintenance and rehabilitation of that housing.
5. The City shall work to protect neighborhoods from harmful and unnecessary vehicular traffic.
6. The City shall encourage a mix of retail, office, professional, wholesale, and non-polluting industrial uses within Claremont.
7. The City shall preserve and promote viable commercial centers.
8. The City shall encourage development that will broaden the local tax base, create employment, and benefit the citizens of Claremont.
9. The City shall require Specific Plans for commercial/industrial sites that will have a significant impact on the city or on surrounding developments.
10. The City shall work with owners and developers to create viable projects.
11. The City shall encourage rehabilitation or redevelopment of designated commercial/industrial areas.
12. The City shall encourage commercial land uses that relate to residential uses and that may be located adjacent to residential areas.

### Redevelopment Element

1. The City shall encourage the preservation, maintenance, enhancement and reuse of existing buildings whenever feasible.
2. The City shall require a high quality of design which stresses imagination and initiative and provides development which is compatible with existing neighborhoods.
3. The City shall encourage the use of Specific Plans to provide innovative development and redevelopment.
4. The City shall encourage development of multiple-family residential housing within redevelopment areas to help meet the needs of a varied population.
5. The City shall encourage the retention and renovation of existing residential structures whenever feasible.
6. The City shall encourage redevelopment which will maintain and expand services to residents, create employment opportunities for local residents and broaden the tax base consistent with overall land use policies of the City.
7. The City shall encourage economic and social activity within the Village consistent with the maintenance of a small-scale, small town atmosphere and image.
8. The City shall require that adequate parking be provided in and adjacent to the Village.

### Historic Preservation Element

1. The City shall incorporate the protection of architectural, historical and archaeological resources in the immediate and long-range planning process of both public and private actions throughout the City.
2. The City shall encourage preservation and enhancement of existing landscaping, recognizing the importance of tree lined streetscapes to the image of Claremont.
3. The City shall encourage active neighborhood participation in the maintenance of their environment and encourage pride in the quality and character of historic areas.
4. The City shall assure that historical residential areas are maintained as cohesive, healthy neighborhood through effective land use, circulation and architectural review policies.
5. The City shall ensure compatibility of new development with the character of the existing neighborhoods especially where the character of the significant architectural and historic resources are affected.



6. The City shall maintain the harmonious appearance of each historic area and address the transitional areas between residential and commercial areas, the residential and industrial areas, and residential areas and the Colleges.
7. The City shall define historical districts or preservation areas within which all development and rehabilitation shall be reviewed in terms of both site and building design.
8. The City shall ensure that no property listed on the local register or adjacent to a property listed on the local register shall be demolished until all alternatives to saving it have been explored.
9. The City shall encourage recycling and re-use of existing cultural resources through expanding the range of incentives for rehabilitation and restoration.

#### **Community Design Element**

1. The City shall encourage the use of public and semi-public buildings, related features such as walls and gates, and open spaces as strong visual focal points.
2. The City shall pursue beautification programs that preserve and enhance the image of Claremont as a community of neighborhoods.
3. The City shall establish street design programs and coordinate street construction or street improvement projects with other city programs.
4. The City shall preserve all existing mature trees within the street right-of-way and encourage preservation of all mature trees on private property where visible from the street.
5. The City shall promote the installation and maintenance of landscaping in public and private areas according to street type, surrounding architecture, general character of the district and street beautification programs.
6. The City shall promote the use of drought-resistant plants.
7. The City shall mark all entryways with markers which differentiate major and minor entries but also have some recognizable element in common.
8. The City shall make city streets the unifying framework of the community through its use of distinctive design treatments.
9. The City shall require developers of new projects creating these urban impacts to offset them by participating in programs to provide greenbelts, open spaces, street landscaping and utility and undergrounding.
10. Through its design review process, the City shall:
  - a. Encourage excellence in architectural design.
  - b. Ensure that new development be a positive addition to the city's environment and not detract from the nature and character of appropriate nearby established development because of architectural style, scale or location.
  - c. Encourage all new development to respect distinctive landforms, significant plants and plant groups in its design.
  - d. Ensure that new development show proper consideration for solar and wind orientation.
  - e. Preserve areas or buildings of historic and architectural significance as physical representations of Claremont's historic and cultural heritage.
  - f. Encourage the restoration and re-use of older structures which contribute to Claremont's character and sense of historic and cultural identity.
11. The City shall promote and protect the unique identity of residential neighborhoods within the city.
12. The City shall pursue a streetscape design program for commercial street frontages along Foothill and Indian Hill Boulevards.
13. The City shall establish a beautification program for the Village, and shall assure that the Village area be designed primarily for the convenience of the pedestrian rather than the motorist.
14. The City, through its review process, shall ensure that site developments show proper consideration for the visual and the functional aspects of the site and the effect on adjoining properties.
15. The City shall develop design standards, not only for the protection of the community at large or neighboring properties, but for the comfort of the employees and their employers.





**Housing Element**

1. Encourage development of new housing in proximity to public services, transportation routes, and other community facilities.
2. Encourage all new development to be of quality design, and that design standards not be compromised in the construction of affordable housing.
3. Require that new housing proposals undergo development review that evaluates proposed sites' relationship to municipal services and surrounding land uses, as well as the architectural compatibility with surrounding neighborhoods.
4. Encourage new housing to be sited in locations that are safe in relationship to traffic and surrounding land uses.
5. Encourage mixed-use development in areas suited for both commercial and residential uses.

**Circulation Element***Non-Motorized Transportation*

1. To provide safe and convenient pedestrian and bicycle facilities for transportation and recreation.

*Parking*

1. To develop and implement joint city/redevelopment agency plans to provide parking that will allow for growth in the Village without harming its physical character.
2. To address the increasing legitimate parking needs in residential areas.
3. To protect residential neighborhoods from the parking impacts of institutions, offices and commercial uses.

**Scenic Route Element**

1. The City shall promote the development of streetscapes which are attractive and interesting with an emphasis on attractive residential, urban, or natural resources rather than on the functional aspects of the road.
2. When appropriate, the City shall encourage the protection of existing stands of trees and other plant material of substantial scenic value.
3. The City shall encourage building height restrictions and setback requirements so as not to obstruct or otherwise impair an exceptional view.
4. The City shall encourage the screening of land uses which conflict with local scenery from roadside view. Visually compatible screening might include the use of landscaping, grading, or attractive fencing.
5. The City shall discourage the use of outdoor advertising within scenic corridors and allow only signs which are compatible to the environment in which they are placed.
6. The City shall encourage structures on public or private properties, which are directly visible from the road, to be well maintained and to present a neat appearance with grounds kept free of trash and other debris.



## 1.9 Relationship to the Land Use and Development Code

The City of Claremont's Land Use and Development Code, Article A – Zoning, provides site specific development and use regulations that govern the size, shape and intensity of development in the Village and uses to which new development may be committed. The provisions of the City Land Use and Development Code apply to the properties in the Specific Plan Area and supplement the regulations stated in the Specific Plan and Design Guidelines. The Specific Plan development standards and design guidelines are consistent with, and further, the City Land Use and Development Code. In such cases where the Specific Plan development standards and Land Use and Development Code standards conflict, the Specific Plan development standards shall apply. Further, where properties are located within the existing Village Plan and also this Specific Plan, the provisions of this Specific Plan shall supersede those in the previously approved Village Plan.

The Specific Plan planned land use and zoning maps are found in Figures 5 and 6 in Section 2.0. The most significant changes in land use and zoning involve the creation of new zones which are mixed-use zones containing a special list of allowable uses that are compatible with the Village environment and the phasing out of industrial uses.

In many cases, the Specific Plan supplements the Land Use and Development Code. In such cases, the City Land Use and Development Code is referenced for detailed standards. This is particularly true for sign requirements. Since the City's Land Use and Development Code involves comprehensive sign standards, sign standards are not emphasized in this Specific Plan, but placement and creative use of signs is detailed.

## 1.10 The Design Review Process

This section of the Specific Plan describes the City's design review process. As with any development or renovation proposal in the City, an applicant must follow a process in order to complete site and building improvements. These procedures apply to any privately or quasi-publicly held parcel or building within the Specific Plan area. For definition, the applicable study area that is subject to these review procedures and subsequent requirements of this Specific Plan is shown on Figure 1.

### Authority

The City's design review process is handled by the Community Development Department. Anyone considering a development project, a remodel, expansion or change of use within the boundaries of the Specific Plan area should first consult a member of the Planning Division to discuss the Specific Plan land uses, zoning, and development standards and design guidelines. Staff can help explain the City's development procedures, assessing whether or not the project first requires any special permits or discretionary land use approvals, such as a Conditional Use Permit. Staff can also provide an approximate timeline for processing the project/application. Application forms and a complete list of application submittal requirements can be obtained





from the Planning Division. Since the Village Expansion Area is part of the City's redevelopment area, the applicant may also benefit from meeting with a staff member from the Redevelopment Agency.

### Applicability

Design review is generally required for all new construction, exterior modifications and substantial remodels to existing buildings or single and multifamily residences, parking lots and exterior pedestrian areas. Projects must also comply with the standards provided in this Specific Plan (See Permitted Uses and Development Standards Matrix, Tables 2 and 3). The specific provisions of the City's code should be discussed with Planning Division staff prior to submission of an application. For more detailed information regarding the City's land use and development review procedures, please refer to the LUDC, Chapter 6 (Development Application Review Procedures) and Article B (Subdivision Ordinance).

### The Design/Development Review Process

The City's design review process begins with submittal of an application to the Planning Division. Planning Division staff will review the submitted application to make sure it is complete. After the application is deemed complete, staff will prepare a written report assessing the overall design, its consistency with applicable City development codes and standards, and its consistency with the design guidelines and development standards of this Specific Plan. The project will then be considered for approval in a public hearing by the Architectural Commission. Factors to be considered in the review of any application include such things as how the project relates to the natural features of the site, surrounding development and the desired architectural character of the immediate neighborhood as prescribed in this Specific Plan. The reviewing authority will also evaluate the effect the project will have on the visual character of the community, the quality of the experience for those who will use the development, and the quality of life in the Village Expansion Area. For example, if the project involves renovation of a landmark building as designated by this Specific Plan, special design criteria may be required to preserve and enhance the building's historic architectural value.

## 1.11 Environmental Review/CEQA Exemption

In accordance with §65457 (a) of the California Government Code, any residential or commercial project, including a subdivision or any zoning change, (that is undertaken to implement and is consistent with this Specific Plan) is exempt from the requirements of the California Environmental Quality Act (CEQA) and requires no further environmental review (see CEQA Guidelines Section 15182). A Program Environmental Impact Report (EIR) has been prepared and certified as a part of this Specific Plan process. Final Program Environmental Impact Report and Response to Comments, State Clearinghouse Number 1999101114. Therefore, any filing of future development applications shall involve the City





making a Specific Plan consistency determination. Where an application is deemed to be inconsistent with the Specific Plan, subsequent environmental review and Plan amendment(s) may be required.

## 1.12 Project Consistency

All projects approved within the Specific Plan Area, including rezonings, tentative subdivision maps, vesting tentative subdivision maps, public works projects, conditional use permits, or any project requiring permitting shall be consistent with this Specific Plan.

## 1.13 Severability Clause

In the event that any regulations, condition, program, or portion of this Specific Plan is held invalid or unconstitutional by a California or Federal Court of competent jurisdiction, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such provisions shall not affect the validity of the remaining provisions thereof.





## 2.0 VILLAGE EXPANSION LAND USES, DEVELOPMENT STANDARDS & DESIGN GUIDELINES

### TOPICS

- 2.1 Purpose
  - 2.1.1 Preserving the Sense of Place
  - 2.1.2 Devising the Plan/Project Description
  - 2.1.3 Why the Development Standards and Design Guidelines Were Prepared
  - 2.1.4 Architectural Character Summary
- 2.2 Village Expansion Area Planning Principles
  - 2.2.1 The Vision & Overall Principles
- 2.3 Land Use Designations, Zoning, and Allowable Uses
  - 2.3.1 Specific Land Use and Zoning Designations
  - 2.3.2 Specific Plan Zoning Descriptions
  - 2.3.3 List of Allowable Uses
- 2.4 Specific Plan Development Standards and Design Guidelines
  - 2.4.1 Who Will Use the Development Standards and Design Guidelines?
  - 2.4.2 The Architectural/Design Review Process
  - 2.4.3 Site Development Standards
  - 2.4.4 Residential and Residential Mixed-Use Design Guidelines
  - 2.4.5 Commercial Mixed-Use Design Guidelines
  - 2.4.6 Mixed-Use Design Guidelines
  - 2.4.7 Packing House Re-Use Criteria
  - 2.4.8 Ice House Office Re-Use Criteria





## 2.1 Purpose

### 2.1.1 Preserving the Sense of Place

The feeling a person has about a particular place may be derived from their experience while in the place. Buildings, open space, trees, sidewalks, roads, and other elements define a place. Successful commercial and residential Village projects depend on providing services and a place which invites people to return time and time again. The land use designations, zoning, development standards and design guidelines have been prepared to foster the continued evolution of the Claremont Village as a successful place. To date, the Village has provided a comfortable environment for businesses and residents. The intent of this Specific Plan section is to suggest a variety of character-uniting ideas and concepts that when applied to existing and future structures, should help further this “sense of place” in the Village Expansion Area, and also ensure that the Village and Village Expansion Area are unified as a single place.

The Claremont Village is a focal point for providing community services as well as capturing attention from many visitors. The architectural design and planning efforts of the Village Expansion effort are aimed at uniting the Village with the Expansion Area and strengthening visual identity. Creating a place that has identity is equally important as designing a land use program that works from an economic point of view. Thus, the site planning, land uses, landscaping and architecture must work together to create a “place” with a strong recognizable character.

The character of the Village is not defined by a theme or period, but is derived from its pedestrian nature, natural elements (such as views of the San Gabriel Mountains, mature trees, river rock details, granite curbs), eclectic and historic architecture, and small town building pattern, rhythm and scale. The overriding intent is to enrich and preserve the Village. This is more than the quality of the buildings. It’s the quality of life in the Village – its traditional role as a place where people meet each other, and enjoy an ice cream on a sunny afternoon. But the Village must also be economically thriving in order to support a nice physical appearance. The City must assess what it and the businesses are doing or not doing that is causing success of the Village.

The success of the Village has prompted existing businesses to expand and additional ones to locate in the area. This growth has been occurring with City stewardship through many efforts over the years to guide the Village evolution in a way that maintains its character and vitality. With the City’s acquisition of key properties to the immediate west of the Village, it has become increasingly important to chart a path for long-term development of the general area encompassing these properties. The City’s oversight of new development on these properties must be balanced with existing Village uses and surrounding neighborhoods’ evolution. The zoning designations and design guidelines are one facet of a comprehensive Village specific planning effort and are intended to allow for the gradual evolution of the Village as a comfortable, vital, and cultural place.



### 2.1.2 Devising the Plan/Project Description

In order to build upon the fabric of the existing Village yet not jeopardize its qualities that have brought the Village success over the years, a guiding set of Planning Principles was developed. (These Planning Principles are listed in Section 2.1.4 of this Specific Plan). To best establish the framework of how those principles might be applied to the Village Expansion Area, a Planning Principles Illustration was prepared. This illustration, Figure 4 on page 2-5, depicts the general fabric and layout of the planning area, and delineates one concept of how the Village Expansion Area may develop over time.

#### 1. Pre-Existing On-Site and Surrounding Land Uses

Existing land uses within the Village Expansion Area at the time of Specific Plan adoption include commercial/retail and office uses along the east and west side of Indian Hill Boulevard, respectively. Residential uses are located along Bonita Avenue, Oberlin Avenue, and Cornell Avenue, in the northern central portion of the Expansion Area. Industrial buildings (including Rich Product's, a cold storage facility, and the vacant Packing House) are located along the north and south side of First Street as depicted on the Existing Conditions Map, Figure 3. Two are undeveloped parcels located at the southwest corner of First Street and Indian Hill Boulevard and additional undeveloped land is located at the western terminus of First Street. In addition, the City Yard is located on the north side of First Street, between Cornell Avenue and Berkeley Avenue.

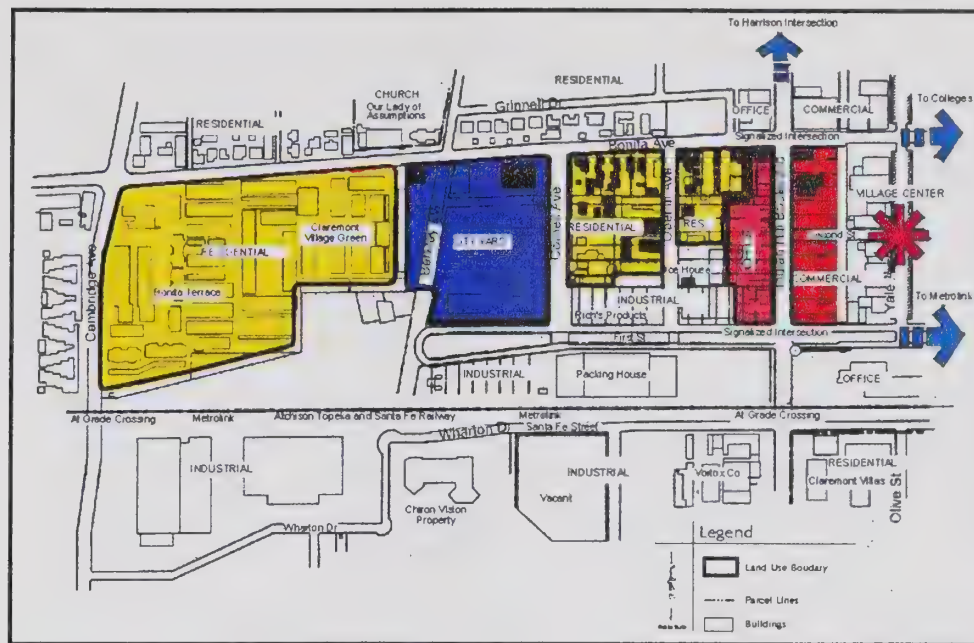
#### 2. Project History

Historically, the project site was used for industrial purposes associated with the citrus industry. The project site originally included three packing houses, two ice houses, and small residential structures occupied by citrus industry workers. As the population in Claremont began to grow rapidly in the 1950s, the citrus fields gave way to new waves of residential development, and by the early 1970s, all of the citrus-related uses were gone. In response to the declining packing industry activity, the City of Claremont chose to incorporate the project site into the Village Redevelopment Project Area.

A 1989 economic development study prepared for the City of Claremont concluded that the Village Expansion Area represented one of the strongest commercial locations in Claremont, and that the area was economically underutilized. In 1996, the Claremont Redevelopment Agency began acquiring substantial land in the Village Expansion Area, and began being solicited by private sector developers to develop portions of this land. As a result, the need for development planning, zoning, development standards and design guidelines for the Village Expansion Area became necessary.







Pre-Existing Conditions in the Planning Area

Figure 3

### 3. Project Description

The proposed project entails the implementation of the Village Expansion Area Specific Plan, which is intended to serve as the long-term development plan for the area, as well as the applicable land use and zoning. Accordingly, a General Plan Amendment and Zone change was adopted concurrently with the adoption of the Specific Plan. Implementation of the Village Expansion Area Specific Plan will allow for a mix of uses within the project boundary, including commercial/retail, office, residential, visitor-serving, and parking uses. Figure 4, the Planning Principles Illustration, identifies the general category of uses, relationship of uses, and organization of uses envisioned by the Specific Plan at build-out. The various land uses within the Village Expansion Area have been redesignated from their previous General Plan designations (see Section 2.2) to Public, Residential Medium Density (6-15 du/acre), and Mixed Use as shown in Figure 5, page 2-20 of this Specific Plan.

With respect to the zone changes for the Village Expansion Area, zoning designations on the City-wide Zoning Map have been changed to a new zoning category ~ Specific Plan 8. Specific uses provided in the Specific Plan 8 zoning category are then identified in this Specific Plan and include Commercial Mixed Use (CMX), Residential Mixed Use (RMX), Public Facilities (PF), Residential (R), and Mixed Use (MX), as illustrated by the Specific Plan Zoning Map, Figure 6, page 2-21 of this Specific Plan.





The CMX zone is intended to be consistent with the Village Overlay District zoning with some subtle use modifications, which requires retail uses adjacent to the street on the first floor and permits residential lofts and office uses above. The RMX district encompasses the northern portion of the project site, where the existing structures are primarily residential. Similar to the CMX district, the RMX district would also allow mixed-uses, including residential, office, studio, and other artisan-type uses; however, the RMX district would not restrict any of the permitted uses to certain floors or portions of buildings, as required by the CMX zone. Commercial retail uses are not permitted in the RMX zone.

The R zone is designated for new residential uses, and would not allow mixed-uses. The MX zone is designed to provide flexibility in the establishment of housing, visitor-serving, or commercial retail uses on the Richs' Products site and the west wing of the Packing House. The PF designation allows for the long-term establishment of the existing police and fire stations.

Specific land uses proposed for the Village Expansion Area are summarized in Table 1. This table reflects pre-existing uses, recycled uses, (under-developed properties that could accommodate additional renovation or development) and new uses, and sets forth a maximum build-out scenario.

TABLE 1  
EXISTING AND PROPOSED LAND USES  
VILLAGE EXPANSION AREA (AT BUILD-OUT)

Use Type	Proposed Area (square feet, s.f./units)			Total
	Existing Area	Recycled	New	
Commercial/Retail	12,674	11,074	126,055	137,129
Theater	0	0	16,500	16,500
Office <sup>1,3</sup>	27,922	17,589	59,345	76,934
Residential <sup>1,2</sup>	42 units	32 units	108 units	140 units
Residential Live/Work Loft <sup>3</sup>	0	0	53 units	53 units
Industrial <sup>4</sup>	260,112	0	0	0
<b>TOTAL <sup>3</sup></b>	<b>300,708</b>	<b>28,663</b>	<b>201,900</b>	<b>230,563</b>
<b>TOTAL <sup>3</sup></b>	<b>42 units</b>	<b>32 units</b>	<b>161 units</b>	<b>193 units</b>

General Note: Rich's and Packing House site development assumptions include 37.5% Retail, 37.5% Office and 25% Residential Live/Work Loft.

<sup>1</sup>Assumes conversion of 25% of existing residential to office in RMX Zone.

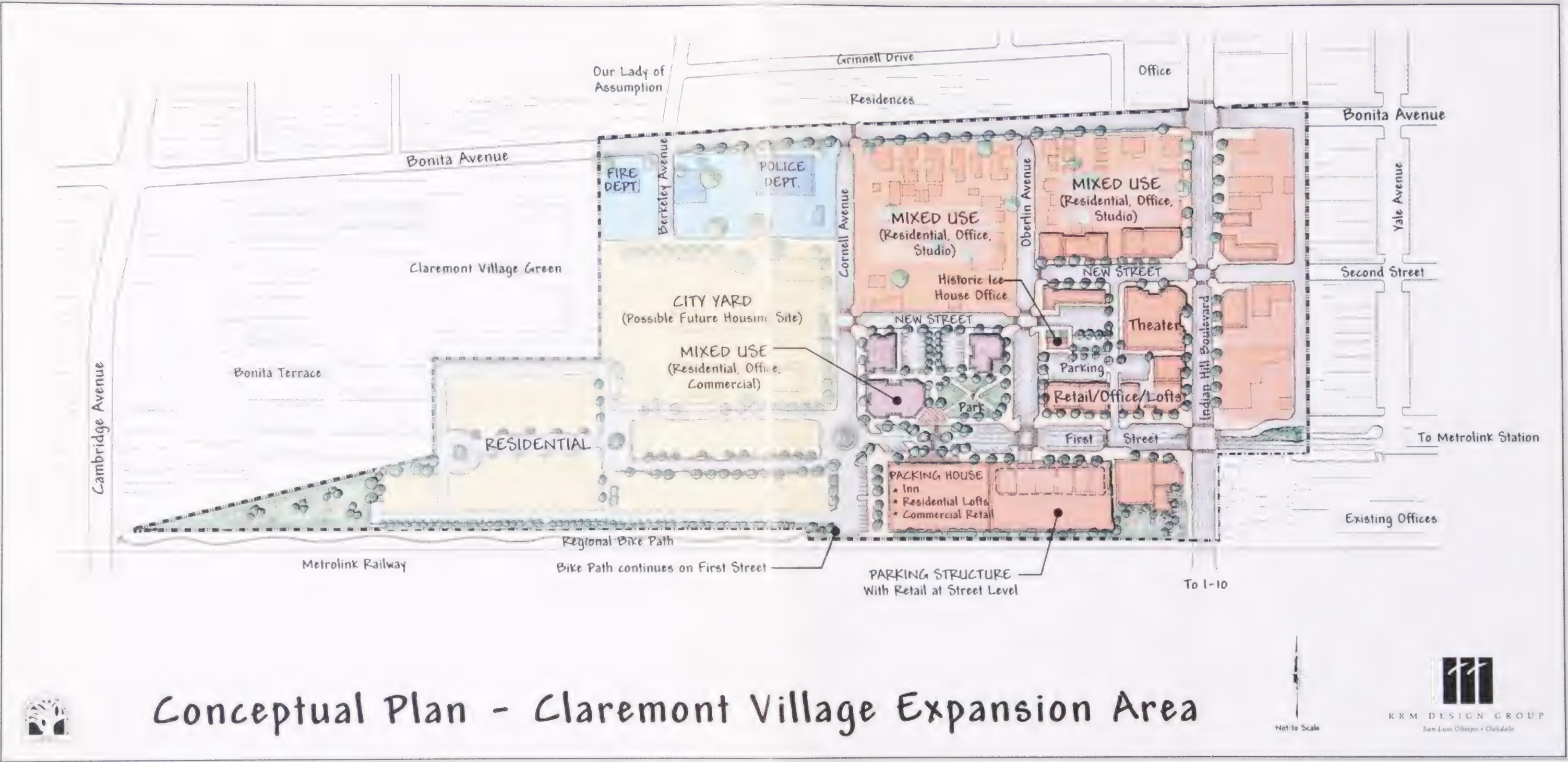
<sup>2</sup>Assumes 25% of new infill residential development in RMX Zone, future relocation of City Yard, and residential build-out.

<sup>3</sup>Assumes 60% of ground floor retail space in CMX has second story, 75% of second story is office uses and 25% of second story is live/work lofts.

<sup>4</sup>Includes the Ice House (32,696 s.f.), Rich's Product's (27,676 s.f.), 607 W. First Street (4,740 s.f.), the City Yard (25,000 s.f.) and the Packing House (170,000 s.f.)







Planning Principles Illustration

Figure 4







### 2.1.3 Why The Development Standards and Design Guidelines Were Prepared

The Village Expansion Area development standards and design guidelines have been prepared to address the major design components for the expansion of the existing Village. Each of the development standards and design guidelines serves to steer successful new development or renovation, and has been crafted to implement certain goals and objectives that can be considered cornerstones. At the same time, the guidelines are intended to be flexible enough to encourage creativity and innovation in design. These cornerstones, as identified below, are essential elements of the Village Expansion Area strategy and planning effort.

- Village-like Atmosphere - Site plans, architecture and landscapes should be organized in a way that emphasizes Village characteristics. (See Section 2.1.1)
- Pedestrian Spaces - All projects should emphasize a pedestrian friendly atmosphere and scale. Spaces such as plazas, paseos, and walkways should be well detailed for the pedestrian and go beyond simple functionality.
- Indian Hill Boulevard - To create a walkable street which incorporates pedestrian scale and streetscape amenities, functional auto circulation, and safe street crossings, new intersection configurations should be considered to bring the Village and Village Expansion Area together.
- Connections - The Packing House site should be developed with the requirement to provide pedestrian and vehicular linkages to adjacent properties. Connections should be developed with ample landscape and focal points wherever possible. Connections should be treated with quality design details rather than appearing as a separate service route. In contrast, surrounding development should provide pedestrian and vehicular linkages to the Packing House so that this central, dominate architectural element is an integral component of the street and building network.
- Corner Buildings - To create a stronger street presence, corner buildings and spaces should be subject to special design criteria. (See Design Guidelines section)
- Materials and Detailing - Buildings and outdoor spaces should be constructed with quality materials and detailing. Significant attention paid to the character of building and landscapes will increase the likelihood of creating a successful place.
- Parking - Parking lots should not dominate the street scene. Convenient and ample parking should be located away from the streets.



### 2.1.4 Architectural Character Summary

The purpose for creating the Village Expansion Area development standards and design guidelines is to implement the City's goal to preserve and enhance the character of the Village and to extend it to those sites where development opportunities exist in the Village Expansion Area. The development standards and design guidelines in this Specific Plan are intended to aid applicants and the City as they design and review future projects (both new construction and renovation) within the Village Expansion Area.



## 2.2 Village Expansion Area Planning Principles

Common ground rules for establishing the design fabric for the Village Expansion Area were achieved through formulating a set of Planning Principles at the outset of the Village Expansion Area planning effort. These Planning Principles were intended to guide the planning process in the development of the Specific Plan. It was also intended that these principles create a fluid set of guidelines that evolve throughout the planning process as new information becomes available. These Principles outline the vision, goals and objectives used to develop the Specific Plan and development standards and design guidelines. Specifically, the Planning Principles include policies on preferred types of land uses encouraged in the Village Expansion Area, economic and financial considerations, pedestrian/bicycle/vehicular circulation, parking, and architectural style and image.

### 2.2.1 The Vision & Overall Principles

To expand the Village to the west side of Indian Hill Boulevard by adding new retail, commercial, and housing opportunities while, at the same time, preserving the economic viability of the Village; to use the character, uniqueness, and pedestrian orientation of the Village as the inspiration for the Village Expansion Area; and to provide for uses and amenities that add to, and cannot be accommodated in, the existing Village.

The vision for the Village Expansion Area is based on Claremont's market niche, economic health of the City, and the principles of new urbanism.

#### The Goals

1. To provide effective guidelines and standards for the location, amount, type, and quality of new and expanded development in the Village Expansion Area.
2. To expand business and residential opportunities in the City and provide amenities to attract shoppers, visitors, students, and residents.
3. To enhance the economic base of the City.
4. To be sensitive to and mindful of historic roots of the City.
5. To develop additional public plazas, open spaces, and landscape/water features to serve as points of interest for the community.
6. To plan for appropriate land uses in the Village Expansion Area that take advantage of the proximity to the transit facilities, including the Metrolink station.

#### Land Use

1. The Village Expansion Area should include a mix of land uses, including, for example, retail, entertainment, residential, office, and artisan/craft studios (e.g. microbrewer, baker, glass blower), with an emphasis on uses that produce income for the City.





2. Live-work situations should be permitted and encouraged (e.g. artisan/craft studios, home offices).
3. Land uses should be complementary and compatible with those in the Village; however, they should not mirror those in the Village.
4. Land uses should contribute to an overall interesting and inviting pedestrian atmosphere.
5. A small inn or bed and breakfast should be encouraged.
6. A small four- to six-screen movie theater should be encouraged.
7. Other uses that should be encouraged are bookstores, art studios/galleries, shops for young adults, medium-size specialty grocery store, sports bar/pub, educational-type retail, and newsstand.
8. Industrial uses are not compatible with the desired land uses in the Village Expansion Area. Existing industrial uses should be phased out over time.
9. Residential and office uses are encouraged above ground floor commercial uses.
10. Businesses and fun activities that take place in the evening should be encouraged, especially those that support one another, such as dining, leisure/entertainment, movies, and art galleries.
11. In keeping with the Village Design Guidelines, existing parking lots at the eastern corners of Indian Hill Boulevard and Second Street should be developed with buildings fronting the street edges and the existing parking relocated elsewhere.
12. New residential development located west of Cornell Avenue should be built at medium densities. Generally, the residential uses should cater to people who want to live near the Village and be easily accessible to public transit.
13. Police and Fire Department facilities should remain.
14. An economic feasibility analysis for adaptive reuse of the Packing House should be prepared and a testing of the marketplace for potential reuses should be conducted as part of the Specific Plan process. This information will be key in determining the ultimate disposition of the Packing House.
15. The center section of the Packing House is the most important historically and offers the greatest potential for reuse.
16. The City Yard should be relocated.

#### **Economic/Financial**

1. The Village Expansion Area should generate new tax revenues.
2. The Village Expansion Area should economically enhance the entire Village.





3. The Village Expansion Area should provide ownership opportunities for small businesses.
4. The cost of development and maintenance of uses in the Village Expansion Area should not have a net negative impact on the City's general fund.
5. City fees should not be waived for development in the Village Expansion Area, except as specified in the City's Land Use and Development Code.
6. City and Redevelopment Agency owned property should be sold for less than market value only if the proposed project:
  - a) Furthers the vision and goals for the Village Expansion Area; and
  - b) Is not financially feasible without assistance.

### **Pedestrian and Bicycle Circulation**

1. Safe, effective, attractive, and inviting pedestrian connections across Indian Hill Boulevard should be included.
2. Pedestrian ways should provide direct linkages to transit facilities.
3. Pedestrian connections to residential neighborhoods adjacent to the Village Expansion Area should be enhanced.
4. Improvements that promote pedestrian traffic should be encouraged and should be ADA compliant, such as wide sidewalks, public seating, clearly defined crosswalks, pedestrian ways, adequate lighting, fountains, landscaping, and plaza areas.
5. The Specific Plan should incorporate the commuter bike route as designated by the San Gabriel Valley Sub-regional Master Bikeway Plan and evaluate providing linkages to other bicycle routes in the City.
6. As the northern border to the Village Expansion Area, Bonita Avenue should be made more pedestrian friendly.

### **Vehicular Circulation**

1. Residential neighborhoods both within and adjacent to the Specific Plan area should be protected from intrusive and unnecessary vehicular traffic.
2. A new design for First Street in the Village Expansion Area should be developed which can transition to the First Street segment east of Indian Hill.
3. A better signal system at the intersection of Indian Hill and First Street should be designed.
4. Circulation must be designed to address fire and safety access and enforcement.





5. Second Street should be extended in across Indian Hill Boulevard at least to Oberlin Avenue to create an additional link to the existing Village, and reduce the size of blocks consistent with the pedestrian grid pattern of the Village.
6. Alleys should be considered that allow for back entry into buildings, service accessibility, and access to mid-block surface parking, similar to the existing Village.
7. Large truck traffic should be limited to retail deliveries.
8. Vehicular connection of First Street through to Cambridge Avenue is not practical; emergency access from Cambridge to the western end of the Village Expansion Area should be explored; and a pedestrian path should be constructed to provide access to the Village Expansion Area from the western border at Cambridge.

### **Parking**

1. The Village Expansion Area should provide sufficient parking to accommodate all its uses (i.e. "Park Itself").
2. The Village Expansion Area should consider providing overflow parking for the existing Village. Parking needs for the existing Village and the Village Expansion Area should be considered as a whole.
3. Both on-street angled, parallel, and inside-the-block parking should be considered, consistent with the pattern in the existing Village.
4. Parking structures, if built, should be located in the interior of blocks or along the Metrolink tracks. They should have retail uses on the first floor.
5. New development should incorporate, as needed, effective parking management ideas—such as employee/merchant shuttle programs, validation programs, shared parking agreements, shuttle programs with the Colleges, parking fees, and parking meters.
6. Creation of a parking improvement district should be explored to pay for and maintain any parking structures and maintain surface parking areas.
7. The City should strive to maintain public ownership over parking lots.

### **Architectural Style/Image and New Development**

1. In commercial areas north of First Street, building facades should be similar in scale to those that generally exist north of First Street in the existing Village.
2. New development should be a mix of architectural styles.
3. Building heights should be varied, not to exceed three stories.





4. The City should encourage the preservation, maintenance, enhancement, and reuse of existing historic resources whenever feasible. Historic building reuse should consider economic feasibility.
5. The City should require a high quality of design that stresses imagination and initiative.
6. The Specific Plan design guidelines should build on the goals of the Community Design Element and the Village Design Guidelines.
7. New buildings and remodels of existing structures should reflect the materials and design as set forth in the Specific Plan Design Guidelines.
8. Elements representing historic citrus industry should be considered in the overall design fabric of the Village Expansion Area.

### **Implementation**

1. The Specific Plan will be the zoning for the Village Expansion Area. It will be the long-term guide for future development in the Village Expansion Area. It will establish the framework both for private sector development and for public improvements, such as street, sidewalks and parks.
2. The Specific Plan should include design guidelines that address entirely new development as well as incremental changes to existing development, to ensure compatibility throughout the plan area.
3. Public improvements should be constructed in conjunction with new development.

### **Housing**

1. Provide a living experience where you can live, shop, walk to train, etc.
2. Provide a residential base to help support the expanded Village.
3. Streets should be as narrow as possible and tree lined. Features to encourage walking must be incorporated. Gated communities are not preferred.
4. Create a neighborhood that will age well, age gracefully and retain value.
5. The street pattern should be compatible with the existing grid. However, circulation must be designed to meet fire and safety access.
6. New housing development should emphasize a pedestrian-oriented strategy with garage access off alleys. Front porches rather than garage doors should dominate the streetscape.



## 2.3 Land Use Designations, Zoning and Allowable Uses

The City's General Plan land use goals and policies have been listed in Chapter 1.0 of this Specific Plan. This section of the Specific Plan contains a detailed description of each zoning category (referred to as Specific Plan Zoning) and followed by Table 2 designating the allowable uses within each zone and whether establishment of the use requires administrative or conditional approval.

### 2.3.1 Specific Plan Land Use and Zoning Designations

#### 1. Specific Plan Land Use Designations

In order to accommodate the special use needs of the Village Expansion Area, a number of new zoning districts have been created, as described below. These new zones can be accommodated by using existing General Plan land use designations, though the entire Village Expansion Area requires re-designation. The land use categories for the Village Expansion Area Specific Plan are divided into three districts as depicted on Figure 5, and include; Mixed-Use, Public and Residential Medium Density.

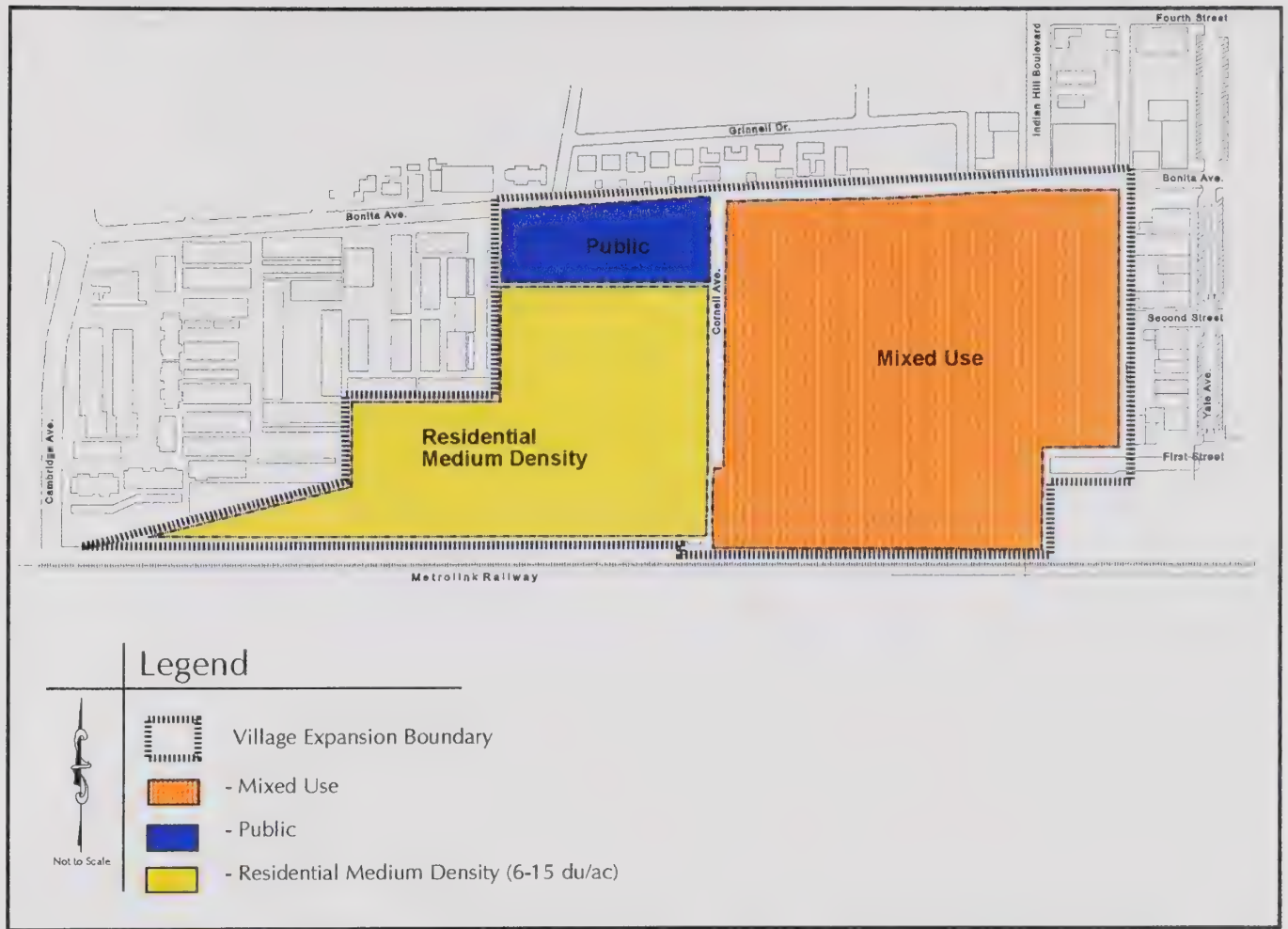
#### 2. Specific Plan Zoning Designations

The Specific Plan zoning designations in the Village Expansion Area are shown in Figure 6 and are based on the premise that the new zoning categories allow for a mix of compatible uses enabling greater flexibility in choosing Village uses over the life of the Specific Plan. While the overall intent is to allow mixed-uses in the Village Expansion Area, there are some exceptions to the mixed-use zones. These exceptions include the Residential (R) zone which is limited to residential uses only; and the Public Facilities (PF) zone, which allows general public service and office (specifically police and fire facilities). Interspersed throughout the various zones will be pockets of open space, either in the form of buffers between land uses, or as public parklands.

Three mixed-use zones are planned for in the Village Expansion Area; CMX, RMX and MX. These zones each provide a range of uses that combine residential, office, visitor-serving, and commercial, focused on allowable uses that are traditional to a village environment. A detailed description of each Specific Plan zone is included in the Section 2.3.2. Allowable uses within each zone follow in Section 2.3.3. In addition to the various zoning categories, the development standards matrix, design guidelines, and allowable uses table, special reuse criteria have been developed for two historic sites: the Packing House and the Ice House Office. These criteria can be referenced in Section 2.4 of this Specific Plan.



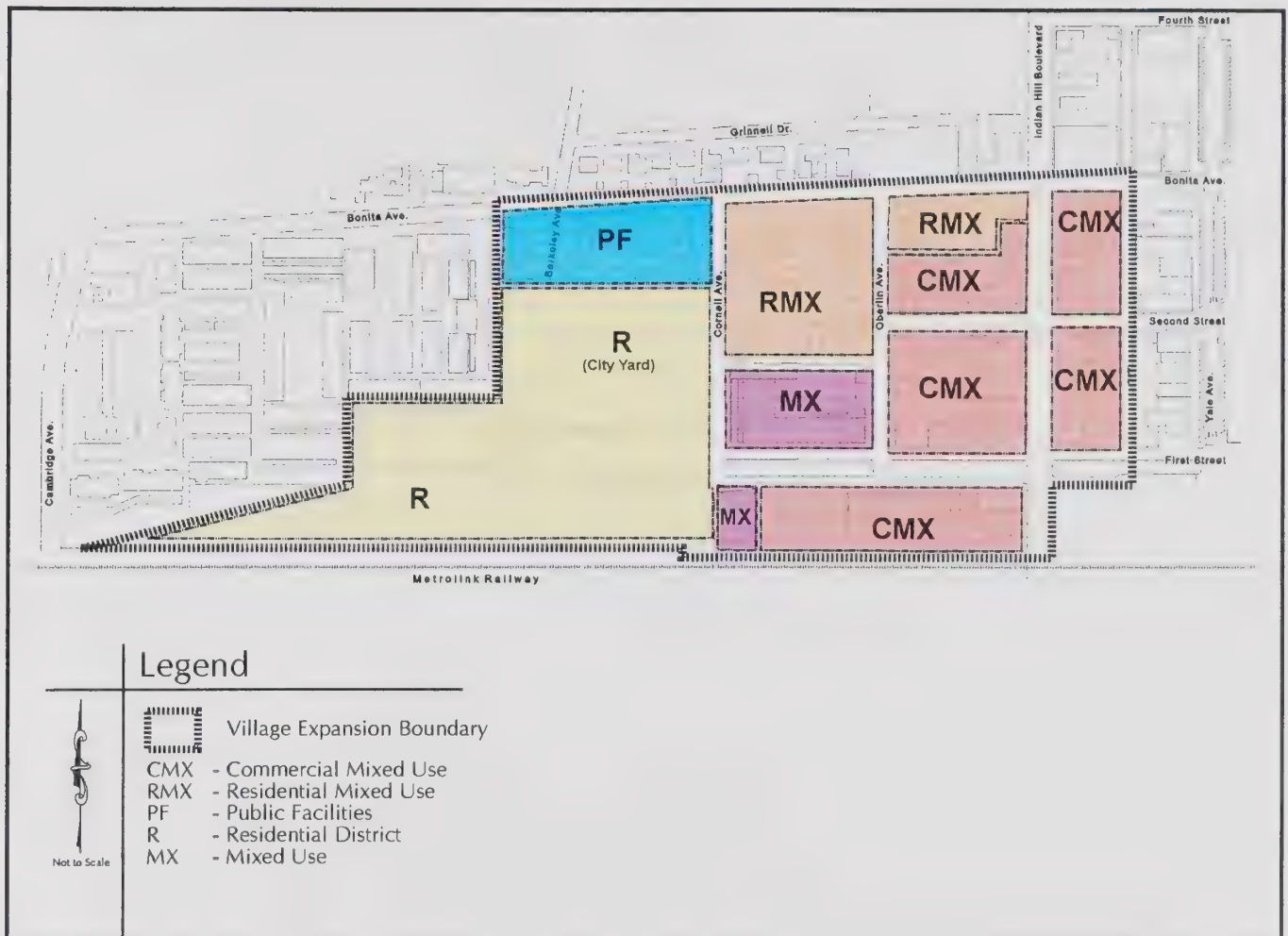




Specific Plan Land Use Designation Map

Figure 5





Specific Plan Zoning Map

Figure 6

### 2.3.2 Specific Plan Zoning Descriptions

The Village Expansion Area contains zones (or sub-areas) with distinctly different design characteristics. The Village Expansion Area has been categorized into five zones to support these differing characteristics. These zones correspond to the zoning/land use designations. The following sections describe the unique nature of these five areas.

- |        |                                  |
|--------|----------------------------------|
| Zone 1 | Commercial Mixed-Use (CMX Zone)  |
| Zone 2 | Residential (R Zone)             |
| Zone 3 | Residential Mixed-Use (RMX Zone) |
| Zone 4 | Mixed-Use (MX Zone)              |
| Zone 5 | Public Facilities (PF)           |

*Packing House & Ice House Office Re-use* - In addition to the four primary zones, special criteria are applied to the Packing House and Ice House Office sites. The purpose of these special criteria is to ensure that certain findings are made and actions taken to protect and promote the re-use of these historically sensitive site/buildings as a part of the Village Expansion Area planning effort.





### Zone 1 - Commercial Mixed-Use

The Commercial Mixed-Use (CMX) zone is intended to provide an extension of the existing Village commercial core. The existing Village pattern is typical of many small town main streets. The buildings are side-by-side, forming a continuous street wall and are not set back from the sidewalk. The storefronts are permeable – that is one can see into and out of them since windows and openings are prevalent. Pedestrian quality is enhanced by carefully maintained public improvements that include shade trees, benches, and other street furniture, wide sidewalks, safe crosswalks, ample landscaping, decorative lights, and split-faced granite curb details. The goal is to extend these characteristics into the Village Expansion Area, and to intensify the pedestrian activity throughout the Village.

New construction and public improvements in this zone should be of similar quality and standards exhibited in the existing Village core commercial blocks. Small blocks, varied storefronts with second story warehouse, offices, apartments or retail support uses is preferred. Emphasis on creating a continuous street wall along Indian Hill Boulevard is essential to bridging the gap that is present between the Yale Avenue corridor and the Village Expansion Area. Public street improvements on Indian Hill Boulevard should focus on “closing” the expanse of the street and making it safe to cross, increasing shade and greenery, and emphasizing the Village entry in a subtle way.

Efforts should ensure that new construction maintains the quality and durability of materials as well as the proper configuration of exterior building elements, such as overhangs, arcades, roofs, signage and awnings. The architecture should fit the San Gabriel Valley climate, customs and local context, (i.e. using native cobble and natural materials). When possible, views to Mount Baldy and the San Gabriel Mountains should be preserved and enhanced.

The distribution of land uses in the CMX zone shall be characterized by ground floor retail with residential, office, and studio uses above. New and refurbished buildings in the CMX district should contain a broad mix of uses, which will likely change a number of times during their usable lifetimes. Residential lofts over retail establishments, not currently found in Claremont, are encouraged in the CMX area to enhance vitality in the commercial core. A special provision to accommodate the continued operation of existing buildings with first floor offices is also provided since these buildings function well and their designs are not particularly conducive to retail uses.

It is the City’s desire to preserve and reuse the existing Packing House in some form. As an anchor to this critical entryway to the City, the redevelopment of the vacant site and Packing House is intended to increase commercial retail activity without detracting from the Village. The pattern of commercial mixed-use/retail development is to be carried along First Street. Ample landscaped setbacks along Indian Hill Boulevard south of First Street will naturally guide visitors into the Village and Expansion Area, and new retail establishments at the corner of First Street will announce entry to the Village environment.



To reinforce pedestrian and bicycle activity along First Street, bike routes and facilities will be accessible. First Street will include angled parking where the right-of-way permits, extending the predominant parking pattern of the Village. A village green and wide sidewalks, street trees, pedestrian crossings and other streetscape amenities will create a street scene compatible with the existing Village, unifying the First Street corridor with the remaining parts of the Village. Emphasis on pedestrian connections between the Metrolink and Depot east of Indian Hill Boulevard and the residential area west of the Packing House will be stressed through properly scaled buildings to the street.

### **Packing House and Ice House Office Reuse**

The citrus industry was a significant element that contributed to the growth and prosperity of Claremont. Even though the industry is no longer active in the Village, it remains a rich part of Claremont's history and roots in the Village formulation.

The Packing House on First Street has historic significance, as does the original office used for Ice House storage operations. Other buildings and sites in the Expansion Area are notable and could have historical attributes, but their historic characteristics should be considered on a case by case basis. The historical merits of the Packing House and Ice House Office are worthy of care thus reuse criteria have been developed that are to apply to any reuse of these buildings and sites. These criteria are described in greater detail in Section 2.4.7 and Section 2.4.8 of this Specific Plan.

In addition to carefully applied design criteria, reuse of the Packing House requires special use consideration in order to ensure that its revitalization is feasible. In order to promote flexibility, all ground and above ground floor uses allowed in the CMX zoning district (see Table 2, Land Use Table) are permitted or conditionally permitted in the Packing House, provided they meet the reuse criteria in Section 2.4.7 of this Specific Plan.

It is possible that a proposal might be presented to re-use the entire Packing House complex, including both of the buildings east and west wings. However, the City recognizes that it may not be possible or feasible in the context of other parts of the Expansion Area Plan to reuse both the center section of the Packing House and either or both of the east and west wings. Nevertheless, the wings of the Packing House should remain in place until such time as either a re-use proposal is recommended for the Packing House that is not plausible unless one or both of the wings are removed, or it is determined that development of a new use adjacent to the Packing House requires the land underlying one or both of the wings in order to maintain its viability.

### **Zone 2 - Residential**

The westernmost portions of the Village Expansion Area are intended for residential (R) development in a pattern typical of the traditional Claremont residential neighborhoods, but in higher densities customary to urban environments. Narrow lots with row-house style product are preferred, with stoops, balconies and porches integrated into the buildings to create a comfortable atmosphere. New development in the residential area should be of high





design/construction quality, improve the area's physical appearance, and enhance the quality of life. Neighborhood residential streets should have smaller street widths, allow for easy walking to the Village, and provide an intimate scale that incorporates plentiful greenery common to historic neighborhood districts. Rear alleys will provide functional spaces for trash and recycling pick-up, emergency access vehicles, and reduce the presence of garages along the street.

Most of the land in the residential portion of the Village Expansion area is undeveloped. Phasing of the residential development will be needed to allow for possible relocation of the City's Corporate Yard. Since the phasing plan envisions residential construction of a first phase that calls for the retention of the City Yard, a buffer in the form of an eight (8') foot block wall will be constructed between the new residential units and the City Yard.

Careful residential housing design and material selection should dictate a sense of establishment rather than sterility that often comes with new construction. Landscape and hardscape amenities should be varied, natural and respect the Claremont region. Pedestrian and bicycle connections between the existing residential neighborhoods to the west and north should be strong. Blocks should be short and incorporate "walkable-streets" to reinforce the Village grid. Open space and small pocket parks should provide ample places for adults to rest and kids to play.

### **Zone 3 - Residential Mixed-Use**

The Residential Mixed-Use (RMX) area is generally bounded by Bonita Avenue, Indian Hill Boulevard, Cornell Avenue and First Street. The area presently contains multiple detached and attached residences and professional offices. A few vacant parcels exist in the area, but most of the land is developed. Many of the existing buildings are well maintained, though some are dilapidated and worn.

The intent to designate this area for Mixed-Use Residential is to promote the continued residential living environment while allowing for conversion of properties to residential-office-studio uses. As demand for small, professional office and studio spaces increase, this area can absorb the independent or professional service group through conversion of residential structures to home occupations or professional office buildings. Through mixing small professional offices side-by-side with residential living, the neighborhood feel of the area is maintained and perpetuated. Large office complexes are not permitted in this area. While mixing residential and commercial uses is permitted in the Commercial Mixed-Use area as well, it is envisioned that "above-retail residential" will be accommodated in the Commercial Mixed-Use area as opposed to "side-by-side residential/office" Mixed-Use allowed in this area.

Most of the buildings have a consistent setback from the street, and conventional front yards are prevalent. The residential frontage along Bonita Avenue between Berkeley and Indian Hill Boulevard creates a comfortable street scene and walking environment, though high vehicle speeds and the wide street make crossing Bonita Avenue difficult. Streetscape design recommendations to improve pedestrian safety for Bonita Avenue include landscaping, travel lane reductions, sidewalks on the north and pedestrian crossings. The internal streets (Cornell Avenue and Oberlin Avenue) reveal a prevailing residential character. The gradual transition





of this area from its current eclectic mix of buildings and uses to a residential and professional “specialty” office environment will build homogeneity in the use mix and street atmosphere.

#### **Zone 4 - Mixed-Use**

Two areas along First Street, west of Indian Hill Boulevard provide a transitional buffer to adjacent, planned residential uses and the historic Packing House. Due to the transitional nature of these two sites, they are designated Mixed-Use (MX) to allow for an array of potential uses. Currently, these two sites have a somewhat different character from the other zones, since one site is home to Rich Products industrial building and the other is located west of the Packing House and along the railroad tracks. The Rich Products’ site is one of the largest buildings in the area, which at the time of writing of this plan, is occupied by an active, thriving industrial use. This building type is a considerably larger size building, typical of the manufacturing trade or a wholesale production facility.

The presence of any building that is not constructed with breaks in building mass, or ornamented in a way that adds to the pedestrian scale of the Village is a deterrent to a friendly street scene. The Rich Products’ facility presents a challenge to extending the Village environment west of Oberlin Avenue due to its size, industrial use and operations. Adaptive reuse of the property is essential to integrating the planned residential uses to the west and a planned mixed-use program in the Packing House. To date, the existing industrial operation uses the public right-of-way for truck parking. With the intent to create a residential neighborhood as an extension to the Village and new commercial/residential uses in the Packing House immediately adjacent to the Rich Products’ site, it will be essential to prohibit the ongoing use of this right-of-way for truck parking. It is envisioned that the Rich’s Food Products site will be transformed into a mix of commercial retail, village green, visitor-serving and/or residential uses, depending upon market forces at hand at the time of Rich Product’s relocation.

The currently vacant property adjacent to the Packing House and the site that is currently constructed as the west wing of the Packing House is also within this zoning category. Proximity of this site to the Village commercial core and it’s location between First Street and the railroad are factors that make the adjacent lot to the Packing House a prime prospect for:

1. Adaptive reuse as a part of the Packing House reuse plan, or
2. As a part of new construction along with the residential development to the west, or
3. As a part of the commercial development north of First Street.

#### **Zone 5 - Public Facilities**

The area zoned for Public Facilities (PF) exists in the northwestern-most portion of the Specific Plan area that is the location of a number of public uses and operations. Presently, the City Police and Fire Department offices as well as the City Yard are in this general vicinity. While it is anticipated that at some time during the long-range horizon of this Plan implementation that the City Yard may be relocated, it will likely remain for some time due to economic conditions. Thus, the City Yard is planned for future residential development verses



remaining as a public facility. The other uses, however, such as the Police and Fire Departments operate efficiently from these existing locations. They are planned to remain and are designated as Public Facilities. In the event that the Police and/or Fire Department facilities are relocated, then the City should consider an amendment to this Specific Plan to redesignate the properties from the Public Facilities (PF) zone to Residential Mixed Use (RMX) or Residential (R).

### 2.3.3 List of Allowable Uses

The Land Use Table (Table 2, page 2-21-27) provides a list of those uses permitted or conditionally permitted in the different zoning designations.

Other uses determined by the Community Development Director to be in similar character to those listed in Table 2 may be permitted or conditionally permitted per the Director's authority. Decisions by the Director may be appealed to the Planning Commission. Conditionally permitted uses are subject to the approval of a conditional use permit requiring Planning Commission approval. Planning Commission approvals may be appealed to the City Council as set forth in Section 1.10 of this Specific Plan.

**Table 2**  
**Land Use Table**

USE	CMX/MX* Ground Floor	CMX/MX 2nd/3rd Floors	RMX	R	PF
<b>1) Administrative/Professional</b>					
Architectural, design, and engineering services	<>	P	P	<>	<>
Art studios with less than 15% sales area	<>	P	P	<>	<>
Attorney/legal services	<>	P	P	<>	<>
Banks, credit unions	<>	P	<>	<>	<>
Remote ATMs (stand alone facilities)	P	<>	<>	<>	<>
Brokerage firms and financial institutions	<>	P	<>	<>	<>
Business management services	<>	P	P	<>	<>
Exhibit halls and galleries with 15% or less retail sales area (For galleries with more than 15% retail sales area - see art gallery under 6) General Merchandise)	<>	P	P	<>	<>
General administrative offices	<>	P	CUP	<>	<>
Government offices	<>	P	<>	<>	P
Insurance and accounting offices	<>	P	P	<>	<>
Medical/dental/electrolysis offices - see use under 9)					
Medical/Health Services					
Real estate, escrow and property management offices	<>	P	CUP	<>	<>
Recording/film studios	<>	CUP	CUP	<>	<>
<b>2) Animal Services</b>					
Feed, tackle supplies for large animals (large animal defined in Muni. Code 6.20.010)	<>	<>	<>	<>	<>
Pet grooming	<>	CUP	<>	<>	<>
Pet hospitals and boarding/kennels	<>	<>	<>	<>	<>
Pet sales and supplies (no outdoor storage)	P	P	<>	<>	<>
Veterinarian office (small animal only with no boarding of pets)	<>	CUP	<>	<>	<>

P = Permitted

CUP = Conditional Use Permit

<> = Not Permitted







Table 2  
Land Use Table

USE	CMX/MX* Ground Floor	CMX/MX 2nd/3rd Floors	RMX	R	PF
<b>3) Alcoholic Beverage Sales</b>					
Alcoholic sales for off-site consumption with or without on-site tasting	CUP	CUP	<>	<>	<>
On-site sales in connection w/restaurant	CUP	CUP	<>	<>	<>
Manufacturing, wholesale and distribution including micro-brewery with no restaurant with limited tasting	<>	<>	<>	<>	<>
Micro-breweries in connection w/restaurant	CUP	CUP	<>	<>	<>
<b>4) Educational/Instructional/Day Care Uses</b>					
Adult day care	<>	<>	CUP	<>	<>
Children tutorial classes	<>	CUP	CUP	<>	<>
Colleges	<>	<>	<>	<>	<>
Elementary, junior and high schools	<>	<>	<>	<>	<>
Music, art, dance, gymnastic, martial arts instruction	<>	CUP	CUP	<>	<>
Nurseries, pre-schools and day care facilities for children	<>	CUP	CUP	<>	<>
<b>5) Food/Restaurants/Eating Establishments</b>					
Bakeries - commercial with ancillary sales - See use under 8)					
Manufacturing/Industrial Uses					
Bakery - primarily retail sales	P	P	<>	<>	<>
Catering services as primary - may include on-site dining facilities	<>	P	<>	<>	<>
Commercial test kitchen	<>	<>	<>	<>	<>
Convenience store - See use under 6) General Merchandise/Retail Trade					
Ice cream, juice, tea and candy shops	P	P	<>	<>	<>
Food markets - See Food/drug and kindred products under 6)					
General Merchandise/Retail Trade					
Micro-brewery in connection w/restaurant - see use under 3)					
Alcoholic Beverage Sales					
Restaurant w/drive through	<>	<>	<>	<>	<>
Restaurants/coffee shops with no drive through facilities	P	P	<>	<>	<>
Restaurants w/entertainment (for entertainment permit see Chapter 5.45 of Municipal Code)	P	P	<>	<>	<>
Retail food including specialty food markets - See Food/drug and kindred products category under 6) General Merchandise/Retail Trade					
<b>6) General Merchandise/Retail Trade</b>					
Antiques retail sales (for antique warehouse see warehouse distribution/operations under 19) Warehouse/Storage)	P	P	<>	<>	<>
Appliance, consumer electronic, computer, and phone/telecommunication equipment retail sales	P	P	<>	<>	<>
Art gallery with retail sales (If retail sales area is less than 15% - see exhibit hall and gallery under 1) Administrative/professional)	P	P	P	<>	<>
Art supplies, framing	P	P	<>	<>	<>
Beauty supplies	P	P	<>	<>	<>
Books and magazines	P	P	<>	<>	<>
Camera and photographic supplies	P	P	<>	<>	<>
Clothing/shoes stores	P	P	<>	<>	<>
Cigar/cigarette/smoke shops	CUP	CUP	<>	<>	<>
Consignment clothing sales	P	P	<>	<>	<>
Convenience stores	CUP	CUP	<>	<>	<>

P = Permitted

CUP = Conditional Use Permit

<> = Not Permitted





**Table 2**  
**Land Use Table**

USE	CMX/MX* Ground Floor	CMX/MX 2nd/3rd Floors	RMX	R	PF
Discount variety, volume liquidation/seconds/cut-rate merchandise, or thrift stores	<>	<>	<>	<>	<>
Equipment sales/rentals w/outdoor storage	<>	<>	<>	<>	<>
Equipment sales/rentals with no outdoor storage	<>	<>	<>	<>	<>
Floor covering	CUP	P	<>	<>	<>
Florists	P	P	<>	<>	<>
Food/drug and kindred products	P	P	<>	<>	<>
Fabric stores	P	P	<>	<>	<>
Furniture, office and home furnishings	P	P	<>	<>	<>
Furniture, office and home furnishings	P	P	<>	<>	<>
Garden supply with outdoor display of plants	CUP	CUP	CUP	<>	<>
General merchandise, specialty, gift, craft items, candles, housewares, and variety (non-discount) stores	P	P	<>	<>	<>
Guns and ammunition	<>	<>	<>	<>	<>
Hardware/home improvement stores	P	P	<>	<>	<>
Health, herbal, botanical stores	P	P	<>	<>	<>
Hobby, toy and game	P	P	<>	<>	<>
Indoor swap meets/concession malls	<>	<>	<>	<>	<>
Interior decorating, linen, and bath stores	P	P	<>	<>	<>
Jewelry sales and repair	P	P	<>	<>	<>
Leather goods and equipment	P	P	<>	<>	<>
Luggage sales	P	P	<>	<>	<>
Music, CD, tape and video sales	P	P	<>	<>	<>
Musical instruments	P	P	<>	<>	<>
Office supplies/stationery/cards	P	P	<>	<>	<>
Outdoor sale or display of merchandise, or provision of services in conjunction with primary use in a building - See Chap. 2, Part 4 for restrictions and special permit requirements					
Pharmacies	P	P	<>	<>	<>
Shoe stores	P	P	<>	<>	<>
Sporting goods and equipment (no gun sales)	P	P	<>	<>	<>
Thrift stores - See Discount variety, volume liquidation/seconds/cut-rate merchandise, army surplus, & thrift stores under General Merchandise/Retail					
Travel agencies	P	P	<>	<>	<>
7) Lodging Places					
Bed and breakfast facilities	CUP	CUP	CUP	<>	<>
Hotels and inns	CUP	CUP	<>	<>	<>
8) Manufacturing/Industrial Uses					
Apparel/textile products	<>	<>	<>	<>	<>
Assembly of products	<>	<>	<>	<>	<>
Auxiliary uses that the planning director finds appropriate and compatible with a permitted manufacturing/industrial use	<>	<>	<>	<>	<>
Bakery-commercial with ancillary sales	<>	<>	<>	<>	<>
Blueprint services	<>	<>	<>	<>	<>
Catalog distribution with no retail sales on the premises	<>	<>	<>	<>	<>
Electronic, optical and scientific products	<>	<>	<>	<>	<>
Food and kindred products	<>	<>	<>	<>	<>
Furniture and household products	<>	<>	<>	<>	<>
Limited retail in conjunction with permitted manufacturing/industrial use (provided retail < 20% total floor area and <20% of gross sales revenue)	<>	<>	<>	<>	<>

P = Permitted

CUP = Conditional Use Permit

&lt;&gt; = Not Permitted





**Table 2**  
**Land Use Table**

USE	CMX/MX* Ground Floor	CMX/MX 2nd/3rd Floors	RMX	R	PF
Laundry/dry cleaners - commercial	<>	<>	<>	<>	<>
Lumber/wood products	<>	<>	<>	<>	<>
Manufacturing as a minor ancillary use to a permitted retail use	CUP	CUP	<>	<>	<>
Metalwork Fabrications	<>	<>	<>	<>	<>
Micro-brewery, winery, distillery - manufacturing, wholesale, tasting - See Manufacturing, wholesale and distribution under 3) Alcoholic Beverage Sales					
Micro-brewery in connection w/restaurant - see this use under 3) Alcoholic Beverage Sales					
Outside operations with permitted manufacturing/industrial uses	<>	<>	<>	<>	<>
Paper products	<>	<>	<>	<>	<>
Photocopying, computer publishing and related personal services - see photocopying and photo development under 11) Personal Services					
Printing/publishing - commercial, large volume/heavy equipment	<>	<>	<>	<>	<>
Research, development and testing of products	<>	<>	<>	<>	<>
Sand and gravel extraction	<>	<>	<>	<>	<>
Stone, clay and glass products	<>	<>	<>	<>	<>
Water resource development	<>	<>	<>	<>	<>
<b>9) Medical/Health Services</b>					
Acute care/walk-in medical services	<>	<>	<>	<>	<>
Ambulance services	<>	<>	<>	<>	<>
Hospitals	<>	<>	<>	<>	<>
Counseling/psychology	<>	P	P	<>	<>
Medical/dental/electrolysis/hearing aids/acupuncture/homeopathy/physical therapy/sports therapy - (For massage see use under 11) Personal Services)	<>	P	<>	<>	<>
Optometry related sales	P	P	<>	<>	<>
<b>10) Motor Vehicles Services</b>					
Automobile body and painting as primary use	<>	<>	<>	<>	<>
Automobile broker offices	<>	SUDP	<>	<>	<>
Automobile car washes	<>	<>	<>	<>	<>
Automobile, motorcycle, and RV sales - new	<>	<>	<>	<>	<>
Automobile, motorcycle, and RV sales - used cars sales permitted only in connection with new car sales	<>	<>	<>	<>	<>
Automobile rental agencies with parking for vehicle rentals	<>	<>	<>	<>	<>
Automobile/motorcycle repair - general, including quick lube, smog check, transmission service	<>	<>	<>	<>	<>
Automobile upholstery	<>	<>	<>	<>	<>
Automobile car alarm and stereo installation (within enclosed building only)	<>	<>	<>	<>	<>
Commercial parking lots and garages	CUP	CUP	<>	<>	<>
Gas/service station (See Chapter 2 Part 3)	<>	<>	<>	<>	<>
Gas station with mini mart, car wash and/or restaurant (See Chap. Part 3 for special standards)	<>	<>	<>	<>	<>
Limousine service with parking for limousine vehicles	<>	<>	<>	<>	<>
Truck, trailer and equipment rentals	<>	<>	<>	<>	<>

P = Permitted

CUP = Conditional Use Permit

&lt;&gt; = Not Permitted

SUDP = Special Use and Development Plan





**Table 2**  
**Land Use Table**

USE	CMX/MX* Ground Floor	CMX/MX 2nd/3rd Floors	RMX	R	PF
<b>11) Personal Services</b>					
Banks, Credit unions, financial institutions - see use under 1) Administrative/profession)					
Barbers, beauty, skin care and nail services	<>	P	<>	<>	<>
Check cashing/deferred deposit or payday advance uses with or without ancillary services	<>	<>	<>	<>	<>
Cemeteries and mausoleums	<>	<>	<>	<>	<>
Dry cleaners/laundry - non-commercial (For commercial see use under 8) Manufacturing/Industrial Uses)	P	P	<>	<>	<>
Fortune Telling - See also Muni. Code Chapter 5.38	<>	<>	<>	<>	<>
Funeral parlors	<>	<>	<>	<>	<>
Locksmith and key shops	P	P	<>	<>	<>
Massage/Acupressure as primary use (See also Muni. Code Chapter 5.36)	<>	CUP	<>	<>	<>
Massage as ancillary use to primary permitted use - less than 40% of gross floor area of business (See Muni.Code Chapter 5.36)	<>	P	<>	<>	<>
Crematory	<>	<>	<>	<>	<>
Pawnshops	<>	<>	<>	<>	<>
Photocopying and photo developing -retail	P	P	<>	<>	<>
Photography studios	P	P	P	<>	<>
Printing/publishing - commercial, large volume/heavy equipment - see this use under 8) Manufacturing/Industrial Uses					
Postal services/mail box rentals	<>	P	<>	<>	<>
Shoe Repair	P	P	<>	<>	<>
Tailor and alterations	P	P	P	<>	<>
Tattoo (See Muni. Code Chapter 9.70)					
<b>12) Public Facilities/Utilities</b>					
Government office uses - See use under 1) Administrative/professional					
Public park	CUP	CUP	CUP	P CUP	CUP
Public maintenance yard and other non-office uses	<>	<>	<>	<>	CUP
Public utility structures	<>	<>	<>	<>	CUP
<b>13) Recreation/Entertainment</b>					
Billiards and pool halls/game and video arcades	<>	CUP	<>	<>	<>
Golf course and driving ranges	<>	<>	<>	<>	<>
Indoor - amusement/recreation/sports and health clubs/skating/batting cages/roller hockey facilities (not within a public park) - For instructional uses see 4) Educational/Instructional/Child Care Uses	CUP	CUP	<>	<>	<>
Outdoor - amusement/recreation/sport club/skating/batting cages/roller hockey facilities (not within a public park)	<>	<>	<>	<>	<>
Public Assembly/auditoriums/meeting halls	<>	<>	<>	<>	<>
Theatres (live stage and movie) and concert halls	CUP	CUP	<>	<>	<>

P = Permitted

CUP = Conditional Use Permit

&lt;&gt; = Not Permitted



**Table 2**  
**Land Use Table**

USE	CMX/MX* Ground Floor	CMX/MX 2nd/3rd Floors	RMX	R	PF
<b>14) Religious Institutions</b>					
Churches and places of worship with accessory uses including garths and columbariums	<>	<>	<>	<>	<>
Monasteries and religious group quarters permitted only in conjunction with a church or place of worship	<>	<>	<>	<>	<>
<b>15) Repair Services</b>					
Antique restoration	<>	CUP	CUP	<>	<>
Equipment and appliance repair	<>	<>	<>	<>	<>
Furniture refinishing	<>	CUP	CUP	<>	<>
Furniture upholstery	<>	CUP	CUP	<>	<>
<b>16) Residential</b>					
Assisted living facilities	<>	<>	<>	<>	<>
Caretaker's or watchman's quarters	<>	<>	<>	<>	<>
Congregate care facilities	<>	<>	<>	<>	<>
Continuing care facilities	<>	<>	<>	<>	<>
Convalescent care	<>	<>	<>	<>	<>
Group care 7 or more people	<>	<>	<>	<>	<>
Single family development	<>	<>	P	SHUP	<>
Multiple family development	<>	<>	P	SHUP	<>
Live/Work lofts	<>	P	P	SHUP	<>
Apartments above commercial space	<>	P	<>	<>	<>
Senior housing <sup>1</sup>	<>	<>	P	SHUP	<>
Student Housing/Dormitory/Group Quarters	<>	<>	<>	<>	<>
<b>17) Service/Non-profit Organizations</b>					
Offices for philanthropic, charitable and service organizations	<>	P	P	<>	<>
Political campaign offices and headquarters	<>	P	<>	<>	<>
Public utility structures - see use under 12) Public Facilities/Utilities					
Second-hand clothing/household goods sales - See Discount variety, volume liquidation/seconds/cut-rate merchandise, army surplus, and thrift stores under 6) General Merchandise/Retail					
<b>18) Temporary and Special Uses (Chap 6 Part 3)</b>					
Christmas tree and pumpkin sales (temporary outside sales)	SUDP	<>	SUDP	<>	<>
Commercial/office use of residential structures	SUDP	<>	SUDP	<>	<>
Fruit stands	SUDP	SUDP	<>	<>	<>
Large family day care	<>	<>	<>	<>	<>
Mobile recycling and reverse vending units	<>	<>	<>	<>	<>
Parking lot sale	SUDP	SUDP	SUDP	<>	<>
Temporary outdoor displays, sales, storage and provisions of services	SUDP	SUDP	SUDP	<>	<>
Temporary parking lots	<>	<>	<>	<>	<>
Temporary use of structures for carnivals, farmers markets, fairs, circuses and religious gatherings	SUDP	SUDP	<>	<>	<>
Temporary use of structures, trailers and facilities related to established uses	SUDP	SUDP	SUDP	<>	<>

P = Permitted

CUP = Conditional Use Permit

SUDP = Special Use and Development Plan

SHUP = Special Housing Use Permit

&lt;&gt; = Not Permitted

<sup>1</sup> 10 units or less only





**Table 2**  
**Land Use Table**

USE	CMX/MX* Ground Floor	CMX/MX 2nd/3rd Floors	RMX	R	PF
<b>19) Warehouse/Storage Uses</b>					
Lumber yard as primary use (for lumber yard as an ancillary use see Hardware/ Home Improvement or Discount/big box under 6) General Merchandise/Retail Trade)	<>	<>	<>	<>	<>
Mini-storage/warehouse facilities - See Chapter 2 Part 6	<>	<>	<>	<>	<>
Outdoor storage as primary use	<>	<>	<>	<>	<>
Outdoor storage as ancillary use to a permitted use in a building	<>	<>	<>	<>	<>
Outdoor storage for gardening/landscape companies	<>	<>	<>	<>	<>
Parcel delivery service	<>	<>	<>	<>	<>
Warehouse distribution/operations	<>	<>	<>	<>	<>
Wholesale building materials w/outdoor storage	<>	<>	<>	<>	<>
Wholesale building materials w/no outdoor storage	<>	<>	<>	<>	<>
<b>20) Wireless antennas - See Chapter 5, Part 6 for permitted antennas</b>					
<p>* Existing ground floor commercial spaces that do not have a display window facing a public sidewalk can be occupied by uses otherwise permitted on the 2nd and 3rd stories in the CMX district until such time as the buildings are removed or modified for retail uses.</p>					

P = Permitted

CUP = Conditional Use Permit

&lt;&gt; = Not Permitted



## 2.4 Specific Plan Development Standards & Design Guidelines

### 2.4.1 Who Will Use the Development Standards & Design Guidelines?

The development standards and design guidelines will serve in several capacities for different people. The following describes some of the potential users.

Lot and Business Owners - will use the development standards and design guidelines to craft redevelopment plans or improvements to existing and future structures. Property or business owners who are most familiar with the design guidelines can best use them in meeting the community's goals.

Design Professionals - Architects, landscape architects, engineers, and other design professionals will work with the lot or business owner in creating an appropriate design. The design guidelines will provide direction for resolving site, building, and landscape design issues in order to assure compatibility with the Village goals. These professionals will use the development standards and design guidelines in conjunction with applicable regulations of the City. It is expressly the ultimate responsibility of the lot/business owner to have each of the design professionals inquire of the City regarding all such regulations and to comply with them.

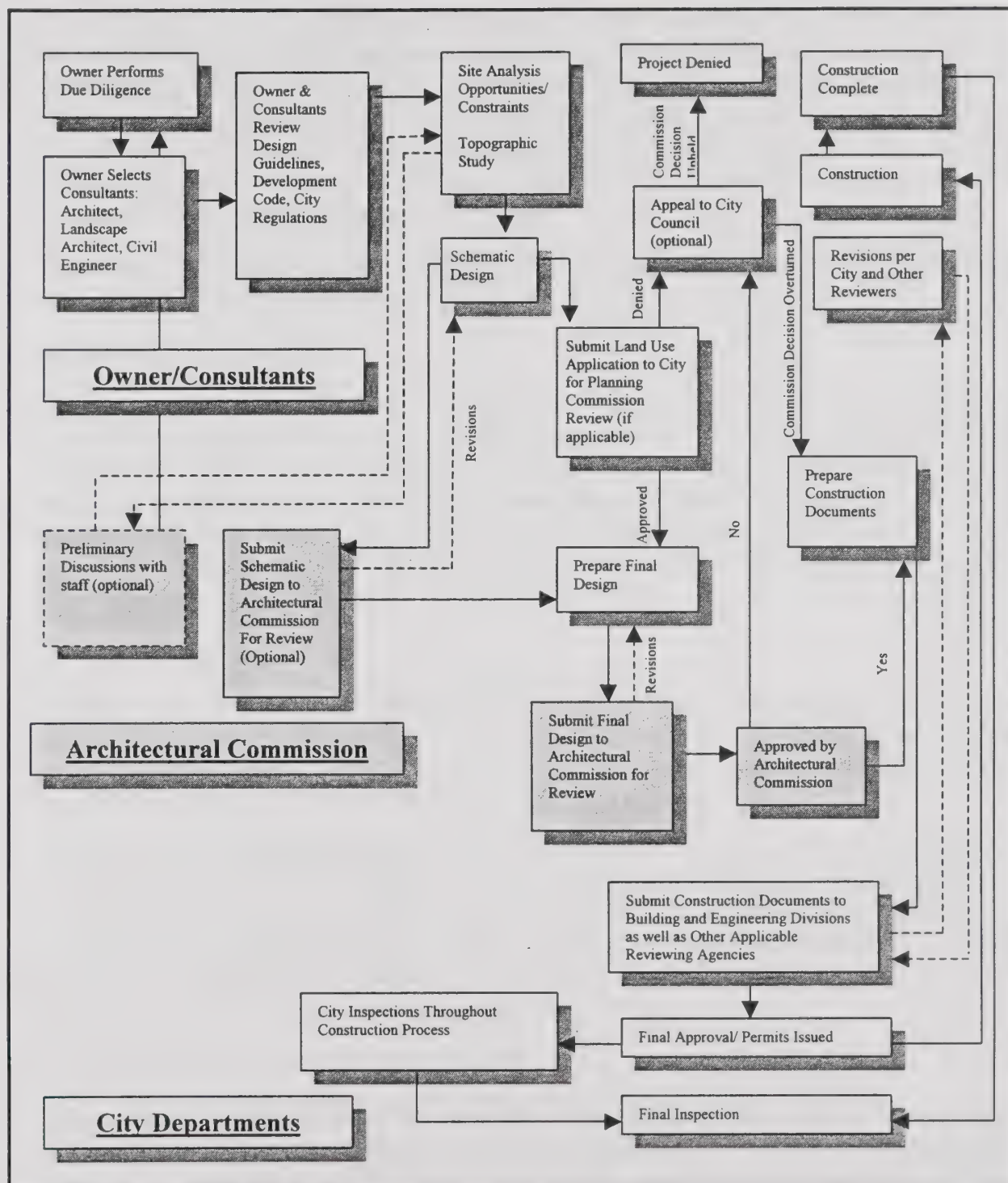
Architectural Commission - Architectural Commission members will use the information in this document as a basis for evaluating all development proposals. These standards and guidelines are intended to simplify the design review process for the Commission while providing an objective review for the applicant.

City of Claremont - The City staff will use the development standards and design guidelines as a checklist to verify that submittals meet these criteria. The City will not issue permits without such compliance.

Contractors and Workmen - All contractors, subcontractors, and workmen are required to perform within the restrictions designated in this document and shall be subject to all penalties imposed for failure to do so. Lot and business owners are responsible for penalties imposed for violations by their contractors, subcontractors, and workmen. Willful or repeated violations may disqualify all such parties from further work within the Claremont Village and subsequent phases of the project.

These users will utilize these development standards and design guidelines in preparing, submitting, and reviewing plans for development proposals. All lot and business owners and their agents will be required to follow the design, submittal, and review process set forth in this document as illustrated in the Architectural/Design Review Process flowchart, Figure 7.





## Architectural/Design Review Process

Figure 7

### 2.4.2 The Architectural/Design Review Process

The Architectural Review process is illustrated by Figure 7. As with all other development-related matters in Claremont, design review is handled by the Planning Division of the Community Development Department, with review and approval by the Architectural





Commission. Anyone considering a development project within the City or Village Expansion Area should first make an appointment to discuss the project and these development standards and design guidelines with a member of the Community Development Department staff. The staff member can help explain the City's development procedures and determine if architectural review is required. The staff member can also describe any other permits or approvals that may be required. To the extent allowed by the City's codes and ordinances, any additional permits will be processed concurrently.

Application forms, and a complete list of application submittal requirements, are available at the Community Development Department.

*When is design review required?*

Generally speaking, design review is required for all new construction, exterior modifications and substantial remodels to existing buildings (both commercial and residential), parking lots, landscaping, exterior lighting and exterior pedestrian areas. Projects must comply with the standards provided in these development standards and design guidelines, as well as those applicable standards found in the Claremont Land Use and Development Code. The specific provisions of this Specific Plan should be discussed with Community Development staff prior to submittal of an application.

*Who does the reviewing?*

To handle the task of design review, the City established the Architectural Commission, consisting of seven members appointed by the City Council who are a blend of design professionals and citizens from the community at large. The Architectural Commission serves as a decision-making authority on all issues of design in the City.

The staff of the Planning Division of the Community Development Department aids the Architectural Commission. Planning staff will prepare a written report to the Architectural Commission providing an analysis of the overall design of the project, its consistency with applicable City development codes, standards, and an evaluation of the project's consistency with these standards and guidelines. Decisions of the Architectural Commission may be appealed to the City Council. A fee is charged for such appeals.

*How does one use these standards and guidelines?*

These private property development standards and design guidelines are composed of distinct sections for the Commercial Mixed-Use, Residential, Residential Mixed-Use and Mixed-Use districts. (Development standards and design guidelines do not apply to the PF-Public Facilities zone. The provisions in the Claremont Land Use and Development Code govern expansions, renovations, or development in this zone). First, each one of the zoning category descriptions should be reviewed for general character statements. After determination of property zoning, the appropriate zone should be referenced for specific site development standards and recommended design guidelines.



*What standards will be used to judge my project?*

Design review, by its nature, involves subjective judgments. That is, in part, why the City prepared these development standards and design guidelines, and also why persons contemplating a development project should meet with City staff to discuss the City's design review process.

In its role as design review authority for the City, the Architectural Commission looks at the entire design of a project, considering such factors as how the project relates to the natural features of the site and to surrounding development. The Architectural Commission will also try to judge the quality of the experience people will have when living, working or shopping in the development, as well as the effect the development will have on visual character of the community and quality of life in Claremont.

When considering an application for design review, staff and the Architectural Commission will ask the following fundamental questions:

- How does the project contribute to the quality of life?

Since buildings and other site development will be a part of the community for years to come, it is important that it contribute in a positive way to the enjoyment of living, working or shopping in the community.

- Does the design of the project suit the proposed site?

For example, does the project accommodate the natural features of the site, such as views, trees, pedestrian connections, open space, etc?

- Does the project respect Claremont's history?

One of the goals of the City's design review process is to preserve and enhance buildings and sites that have historical value by virtue of its architecture, historic association or age. For example, when converting a bungalow into offices, it would usually be unacceptable to replace wood-sash windows with modern materials such as aluminum, and signs would need to respect the style of the building and neighborhood.

- Does the project follow basic principles of good design?

New development should project a sense of harmony, proportion and balance among its various different components, and should relate to its site and surroundings in a functional and pleasing way. Remodels and additions to existing buildings should consider adjacent uses, scale and character of nearby buildings and how people and cars most efficiently move in the Village environment.

- Does the project suit its purpose?

For example, does an apartment project look friendly, homelike and livable? And if families are expected to live in a project, are there safe, usable outdoor areas? If the project is a commercial building, does it look like a place to shop or do business? Is the design functional?





- Is the project architecturally interesting and imaginative?

One of the distinguishing features of the buildings, storefront and spaces in the Village is their individuality and eclectic character. The use of creative design solutions and distinctive materials is encouraged. For example, signs that have unique graphic form, interesting placement on a building or that contain artistic images are encouraged.

- Does the project make good use of the site?

“Good use” of a site implies taking advantage of the opportunities provided by its natural features. For example, are interior spaces oriented to take advantage of views to the San Gabriel Mountains or onto a public space? Are natural topography, trees and other features preserved and protected? Does the orientation of the building and landscaping provide opportunities for passive solar heating and cooling?

- Are materials, forms and other elements of a project suitable for their uses?

Maintenance, longevity and durability are key design considerations in a village environment. Has maintenance been considered in the choice of materials and finishes? Will trees provide shade where and when it is needed? Are ground surface materials safe and practical for walking?

These are not the only criteria to be assessed by the Architectural Commission in considering the design of new development or renovations. Each project and project site is unique and presents its own constraints and opportunities for a good design solution.

### 2.4.3 Site Development Standards and Special Housing Use Permits

Site development standards are necessary to regulate how new and renovated buildings are situated on and in relationship to adjacent sites. As opposed to design guidelines which are *recommended*, development standards are *required* and must be met by any application for development. Table 3, the Development Standards Matrix sets forth the required buildings and site development standards for each zone in the Village Expansion Area. Variances to these development standards are only permitted if approval is granted by the appropriate decision-making body (see Section 1.10 of this Specific Plan for approval authorities). In such cases where properties lie within the boundaries of both the existing Village Plan and the Village Expansion Area Specific Plan, the design guidelines of the Village Expansion Area Specific Plan shall supersede the existing Village Design Plan.

#### Residential (R) Zone - Special Housing Use Permit (SHUP)

The residential component of the Village Expansion Area Specific Plan comprises a significant land area. In order to ensure that the City has the ability to guide the density, site, and architectural design in a way that most appropriately suits the Village, special approval criteria and the requirement for a Special Housing Use Permit have been developed. These special criteria are intended to promote housing product individuality, economic vitality and



compatibility with surrounding uses. The Planning Commission shall have review authority over any and all conceptual site plans and housing types for development in the Residential (R) zone.

### Special Housing Use Permit (SHUP) Requirements

In order to ensure that the City achieves the type of housing product that meets the goals of this Specific Plan, the Planning Commission shall review and make decisions to approve or deny a Special Housing Use Permit for any development in the R-zone. Applications for a Special Housing Use Permit shall meet the intent of the Residential Design Guidelines set forth in Section 2.4.4 of this Plan as well as the requirements of Table 3, Development Standards Matrix. In addition, applications for a Special Housing Use Permit in the Residential Zone shall include properly dimensioned site plans, building floor-prints, elevations of fronts and rears of buildings (at minimum), landscape plans, special fencing and/or patio details, and a lot mix site plan, or any other materials deemed necessary by the Community Development Director. Prior to approval of the Special Use Permit, the following findings must be made by the Planning Commission:

1. The project provides housing for persons who desire an urban/village-like living experience in close proximity to the Village and transit facilities.
2. The project emphasizes a pedestrian-oriented streetscape.
3. The project provides a variety of housing unit floor plans and sizes.
4. The project provides some variety of compatible architectural styles.
5. The project promotes the overall financial feasibility to implement the Village Expansion Area Specific Plan.
6. The project provides a housing type that would enhance the economic base and viability of the overall Village.

### Zone Definitions

For zone definitions, refer to Section 2.3.2 *Specific Plan Zoning Descriptions*.





**Table 3**  
**Village Expansion Area Development Standards Matrix**

The following property development standards shall apply to all land and structures in the Village Expansion Area. These standards are in addition to those contained in the Claremont Land Use and Development Code (LUDC). Where these standards differ from those in the LUDC, the standards in this table shall take precedence. Standards for accessory structures, fences, signs, and parking are contained in Chapter 4 General Development Standards of the LUDC. (Also, see Chapter 4 of the LUDC for requirements for off-site improvements and dedication requirements, visual screening.) Development in the MX district of the Village Expansion Area will be subject to either R or CMX standards, depending on whether the development is residential or commercial.

Standard	R		RMX	CMX
	Detached	Attached		
Minimum Lot Size (sq. ft.)	2100	2100	7000	1750
Minimum Lot Width	30	30	40	25
Minimum Lot Depth	70	70	140	35
Minimum Lot Area Per Unit (sq. ft./unit)	2100	2100	2000	
Maximum Residential Units Per Building	1	10	4	
Maximum Floor Area (sq. ft.)	1,500 + 25% of lot area		3200	
Maximum Floor Area of Second Story (sq. ft.)			1400	
Maximum Floor Area of Accessory Structure (sq. ft.)	420 per story	420 per story		
Minimum Size of Front Porch (sq. ft.)	75			
Minimum Setbacks (ft.)				
Front	10, with average of 12 (lots ≤ 2500 sq. ft.); 12, with average of 14 (lots > 2500 sq. ft.)	10	20	0
Rear <sup>1</sup>	3	3	5	0 <sup>2</sup>
Interior side <sup>1</sup>	0 <sup>3</sup>	5	5	0 <sup>2</sup>
Street side	10	10	10	
Garage (front setback)	20	20	20	
Driveway Length	≤3 or ≥20	≤3 or ≥20	≤5 or ≥20	
Distance Between Buildings (ft.)	9	10	10	0
Building Heights (ft.)				
Primary Structure	25	35	25	35
Detached Garage with Living Area Above	25	25		
Stories	2	3	2	3
Minimum Living Area Per Unit (sq. ft.)		500	400	400
Minimum Private Outdoor Living Area (sq. ft./unit)		100 <sup>4</sup>	100 <sup>4</sup>	
Lot Coverage (% of total sq. ft.)	60%	60%	35%	
<b>Parking Standards</b>				
Loft (in CMX only)	1 space for every 600 sq. ft. of floor area			
Office	1 space per 350 sq. ft. of floor area			
Retail	1 space per 500 sq. ft. of floor area			
Restaurant	1 space per 150 sq. ft. of floor area			
R – detached	2 spaces per unit (enclosed); .25 visitor spaces per unit			
R – attached	2 spaces per unit (enclosed); .5 visitor spaces per unit <sup>5</sup>			
RMX				
Units with ≤600 sq. ft. of gross floor area	1 space (enclosed), .5 visitor's spaces (uncovered)			
Units with >600 sq. ft. of gross floor area	2 spaces (enclosed), .5 visitor's spaces (uncovered)			

**Footnotes:**

<sup>1</sup>Chimneys may encroach up to two feet into rear and side yard setbacks.

<sup>2</sup>Ten (10) foot setback is required when abutting the R or RMX districts.

<sup>3</sup>Minimum interior yard setbacks shall be 4.5 feet unless project is built with a zero lot line configuration either by building on the lot line or the granting of side yard easements. The City may require side yard easements to ensure that each lot has usable private outdoor living area.

<sup>4</sup>For multiple-story apartments or condominiums where units have not ground floor access, and in other cases, where the Architectural Commission finds that provision of the required private outdoor living area is impractical or undesirable, private outdoor living area may be reduced but shall not be less than 50 square feet.

<sup>5</sup>The Architectural Commission may allow credit toward guest parking for adequate on-street parking.

**Definitions:**

**Floor Area:** The calculation of Floor Area shall include the floor areas of all floors of the main residential structure, including the area of an upper level not separated from a lower level by a floor/ceiling assembly, floor areas of attached garages and 50 percent of any living area in a detached accessory structure. Floor Area shall not include the floor area of non-living area in detached accessory structures, porches, patios, decks, balconies and where the floor-to-ceiling height is less than 6-1/2 feet.

**Lot Coverage:** Lot coverage shall include all areas of the lot covered by any buildings, including carports, covered patios and decks, and balconies and decks more than 3 feet above grade; it shall not include uncovered outdoor recreation areas or equipment, uncovered patios, walkways and driveways, pools, and uncovered decks and balconies no more than 3 feet above grade.







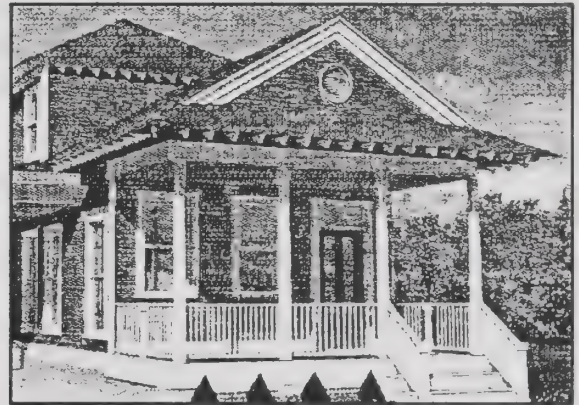
## 2.4.4 Residential and Residential Mixed-Use Design Guidelines

### Topics

- 2.4.4-1.0 Purpose
- 2.4.4-2.0 Applicability
- 2.4.4-3.0 Development Standards
- 2.4.4-4.0 General Design Principles
- 2.4.4-5.0 Site Planning and Design
- 2.4.4-6.0 Parking and Driveways
- 2.4.4-7.0 Architectural Design Guidelines
- 2.4.4-8.0 Utilitarian Aspects of Buildings
- 2.4.4-9.0 Lighting
- 2.4.4-10.0 Landscaping
- 2.4.4-11.0 Signage



*Townhomes and multi-family structures are encouraged in the R and RMX zones*



*Contemporary design incorporating traditional elements*

### 1.0 PURPOSE

The City encourages new development to use designs and an urban form that recall the area's history and small town character. Desirable features include houses on smaller lots oriented toward the street; townhouses designed with appropriate massing and scale to meld with the surrounding single family residences; varied architectural styles; relatively narrow streets and minimum 5 feet wide sidewalks; landscaped parkways between curbs and sidewalks; split stone curbs; non-meandering sidewalks; large trees; the use of alleys and garages located at the rear of the lot; an emphasis on pedestrian, bicycle and public transit options; and the integration of several activities in close proximity, including jobs, schools, housing, shopping and recreational facilities. These features are often referred to as "neo-traditional" because they reflect traditional forms but are also adapted to present day circumstances. The advantages of this design approach include an increased sense of neighborhood and community; reduced emphasis on the automobile and greater use of bicycles, and walking for neighborhood circulation; and a more attractive, aesthetically pleasing streetscape.

The Residential Mixed-Use (RMX) designation permits combining residential and office uses on a site by encouraging home occupations or professional office spaces. This area differs from the Commercial Mixed-Use area, in that it encourages residential uses side-by-side with professional office, instead of second-floor residential units over commercial and office spaces.



## 2.0 APPLICABILITY

The following guidelines address the residential and residential mixed neighborhoods in the north and west portions of the Specific Plan area. The intent of these guidelines is to direct new development, as well as the renovation and expansion of existing housing stock in a way that preserves and enriches the Village character and maintains the character of historic residential buildings in the area.

These guidelines also address concerns over how the newer residential neighborhoods interface with the older residential neighborhoods. They also encourage designs that provide visual, pedestrian, and vehicular linkages.

The guidelines will be utilized during the City's design review process to encourage the highest level of design quality, while at the same time providing flexibility in their application to specific projects, as not all design criteria may be workable or appropriate for each project. Unless there is a compelling reason, these guidelines shall be followed. Generally, the City will not waive a guideline unless the project designers can demonstrate that such a waiver will facilitate an innovative or otherwise preferable design concept.

## 3.0 DEVELOPMENT STANDARDS

The Development Standards Matrix Table 3 outlines the requirements for development in the R and RMX zone in the Village Expansion Area.







## 4.0 GENERAL DESIGN PRINCIPLES

### 4.1 Architectural Character

The intent of the guidelines is to build and maintain a neighborhood with diversity in the architectural styles, building type, materials and details, building square footage, setbacks, lot widths, and landscaping. No one particular architectural theme is being promoted, but rather the emphasis is to promote variety within a traditional neighborhood design framework. In the RMX district, there should be a soft, attractive, and smooth transition between the commercial core and residential neighborhoods to the north and west. Many of the styles and patterns shown in the following pictures and graphic illustrations represent a concept of recommended building elements and details as opposed to thematic images. Caution should be exercised when considering architectural styles or detailing that have recently become popular, but have not yet stood the test of time. In new construction, historic styles that cannot be faithfully replicated should be avoided.

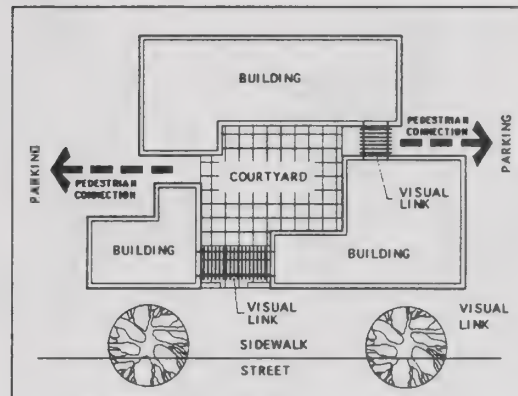


*Variation in building height, forms, architectural styles and ornamentation is encouraged.*

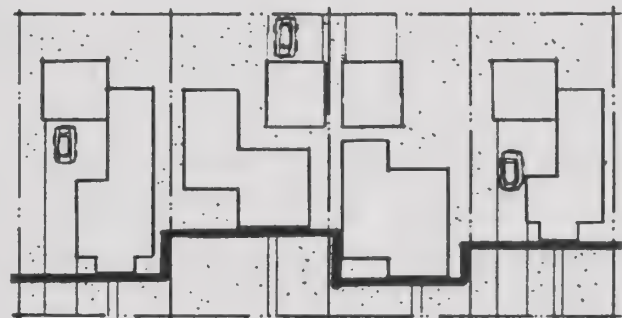


## 5.0 SITE PLANNING AND DESIGN

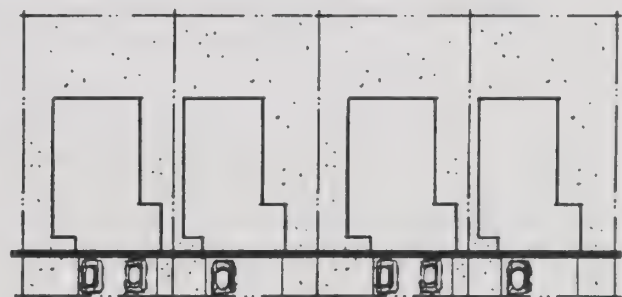
1. Unless impractical due to physical constraints, alleys should be used for access to garages, parking spaces, and for other functions such as garbage pick-up.
2. Front setbacks should vary to provide visual interest and avoid creating an atmosphere common to tract development.
3. Energy conservation should be considered in the orientation of buildings (e.g. solar access, shade control)
4. Consideration should be given for privacy relative to adjoining properties. Orient buildings and decks to maximize views while preserving privacy of surrounding neighbors.
5. Safe and efficient access to usable common open space shall be incorporated into housing developments.
6. Activity areas, windows and balconies shall be placed to avoid negative impacts on neighbors.
7. Houses should be oriented toward the street. Front porches are encouraged.
8. Large multi-family residential projects should be divided into smaller clusters of structures.



*Large projects should be divided into smaller clusters.*

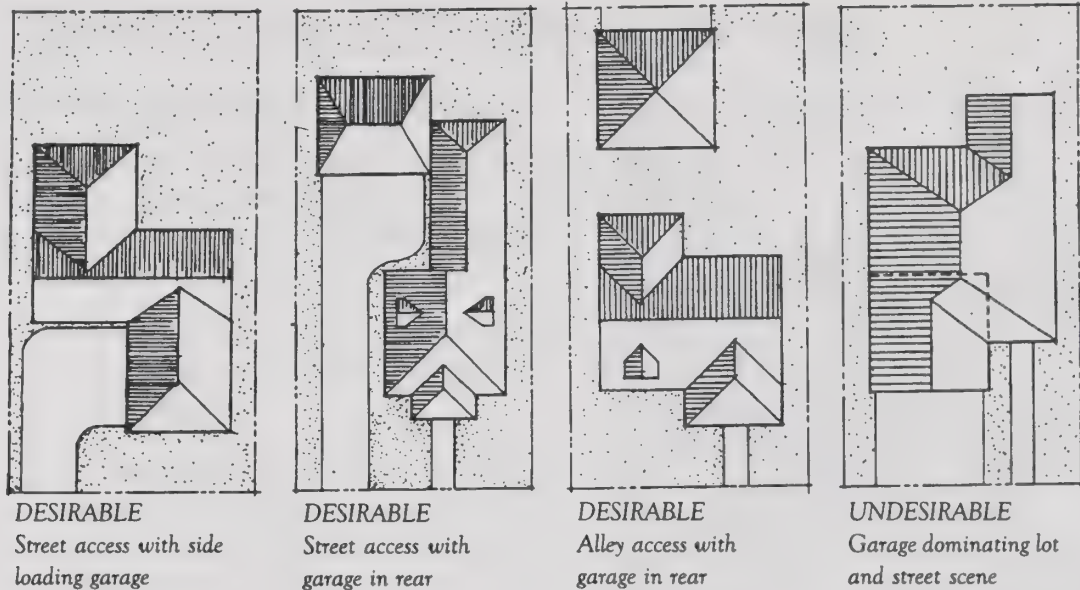


*Varied front setbacks create visual appeal from the street.*



*Avoid a uniform front setback*





## 6.0 PARKING AND DRIVEWAYS

### 1. Sides and Rears of Buildings

- The preferred location for parking is at the rear of lots, accessible from alleys or single-width driveways extending along the side of the lots.
  - Parking areas should be screened from public street views and surrounding residential areas.
2. All parking and driveways in the R / RMX district shall be provided in accordance with Section 434 and 435 of the Land Use and Development Code.
  3. Parking in multi-family residential projects should be visible from the units they serve and located at the rear of the building where possible.
  4. Driveway apron width should be minimized.
  5. Driveway paving treatment should not encroach into the sidewalk.
  6. Alternatives to solid paved driveways such as brick, cobblestone, or interlocking pavers are encouraged. However, such enhanced surfaces may not encroach into the sidewalk.
  7. Glare from large areas of concrete should be reduced by adding an integral color to the mix (earth tones).

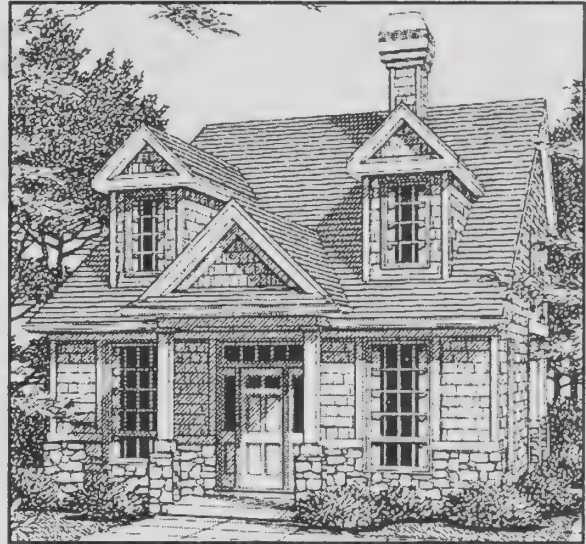




## 7.0 ARCHITECTURAL DESIGN GUIDELINES

### 7.1 Building Mass and Articulation

1. Vertical and horizontal variation should be appropriately implemented in order to add richness and variety to the overall mass.
2. In attached buildings, individual units should be recognizable from the overall building mass.
3. Trellises, pergolas, gazebos and other outdoor structures are encouraged provided they meet the Code requirements with respect to height, placement and construction. Materials and colors should match or complement the main building.



*Building articulation enhances the building facade*

4. Variety in exterior materials can be used to emphasize a building's articulation (see subsection 7.4).
5. All sides of the building, including the rear, should be articulated and given detail.
6. Projections, recesses and overhangs should be employed to provide shadow and depth.
7. Wall and roof planes should be varied and articulated into smaller modules that reduce the overall massing and scale of building and add richness and variety.



*Vertical and horizontal variation should be implemented.*

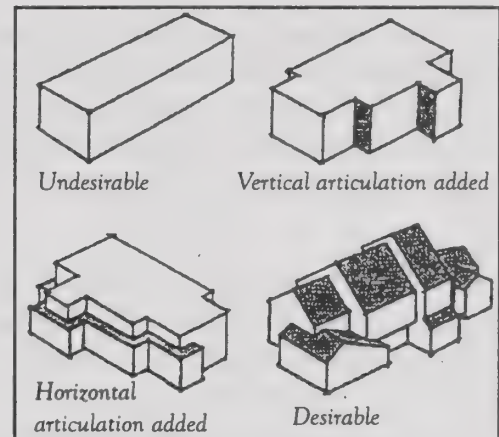
8. Attached residential units should include design elements to add visual interest and to avoid "box-like" appearances. Elements such as balconies, porches, arcades, dormers, and cross gables should be considered. Hipped or gable roofs are preferred to mansard-type roofs.



## 7.2 Building Additions

The following guidelines are primarily intended to address additions to buildings whose existing exterior features are considered worthy of preservation and/or continuation, and may not be applicable in all cases. Proposals for building additions or modifications will be reviewed for appropriateness on a per project basis.

1. Additions to existing structures should be encouraged only on the sides and rear of the existing building.
2. Materials, window shapes, architectural trim, roof form and other architectural elements of additions should match those of the existing building.
3. Incompatible window materials are strongly discouraged.



## 7.3 Building Conversions (Applicable to RMX Only)

1. Conversions of residential structures to commercial use shall retain the residential character of the existing building.
2. Whenever possible, existing structures that display desirable character should be preserved.

## 7.4 Exterior Building Materials & Colors

### 7.4.1 General Criteria

1. Materials and finishes should be true to the architectural style chosen.
2. Careful detail should be taken at the intersections of different materials to avoid awkward transitions. Siding should wrap around outside corners. Changes in siding materials should occur at inside corners of buildings.
3. All stucco wall surfaces should be smooth to prevent collection of dirt, surface pollutants, and surface paint deterioration. Textured stucco is not encouraged unless it is hand troweled or float finished.
4. The roof material selected should preferably be of a darker, non-reflective color provided that energy efficiency standards are met.
5. Generally, no more than three different materials should be used on exterior wall surfaces.





### 7.4.2 Exterior Materials

The following listings of appropriate and inappropriate materials were developed based upon the professional judgement and opinions of those involved in the development of these guidelines. The materials listed as “appropriate” have generally stood the test of time to have desirable aesthetic qualities, while those listed as “inappropriate” have failed to do so. Certain materials have been omitted from either column because their success or failure has more to do with the overall design, rather than the materials themselves.

These listings are intended to provide general guidance, and are not intended to be exhaustive; designers should not feel that they must limit their material palette to the following selections. New materials and refined fabrication methods are continually being introduced (some of which may allow certain materials to be dropped from the “inappropriate” column), and Claremont encourages designers to take advantage of these innovations.

#### Appropriate Primary Wall Surfaces

- Natural Rock
- River Rock
- Cobblestone
- Float finished stucco
- Smooth sand finished hand troweled stucco
- Hand troweled plaster surfaces
- Tile
- Clay brick
- Precast Concrete
- Concrete
- Wood shingles / shakes
- Wood board and batten siding
- Horizontal wood siding

#### Inappropriate Primary Wall Surfaces

- Simulated materials such as cement brick
- T-111 Siding
- Slump stone
- Precision block
- Split face block
- Highly polished materials
- Spray on “Spanish lace” or knock down stucco
- Cultured stone
- Mirror glass and heavily tinted glass
- False stone veneer
- Corrugated fiberglass

The following materials are for residential buildings within the Village Expansion Area.

#### Appropriate Roofing Materials

- Clay tiles
- Wood shingles / shakes  
( Minimum Class B )
- Asphalt shingles
- Concrete roof tiles

#### Inappropriate Roofing Materials

- Maxitile
- Fiber cement units that imitate slate, wood shakes or mission tile.



#### Appropriate Wall Articulation and Accents

- Ornamental tile for accent
- Build up stucco relief
- Prefabricated molding forms
- Inlaid tile
- Painted wainscoting
- Natural stone
- Wrought iron
- Terra cotta

#### Inappropriate Wall Articulation and Accents

- Plastic
- Chrome
- Aluminum windows
- Highly polished materials

#### 7.4.3 Special Treatments (Tile, arches, rails and ornamentation)

1. Existing architectural moldings, ornamentation and other interesting detailing should be retained and repaired or replaced to match the original detail.
2. New development should provide similar ornamentation as existing historic structures in the area to enhance the overall Village theme.
3. Arches, tile inlay, or similar architectural accents should be used to reinforce traditional style.

#### 7.4.4 Exterior Colors

1. Subtle, warm, earth tones such as browns, grays, tans, blues, and greens with complementary accents are appropriate color choices.
2. Simple color schemes involving a maximum of 3 colors are recommended.
3. Blue, orange or light colored roofing is expressly discouraged, as are pastel or bright colored walls/trim.
4. Certain materials, such as stone and brick, have distinct coloring in their natural state and should be thought of as an element of the color palette used.
5. Colors from the entire spectrum of hues should be selected, but the chroma and value of the colors should be limited so that they are subdued, especially on larger buildings.
6. Brighter chroma should be eliminated from the palette for a building, except for accents.



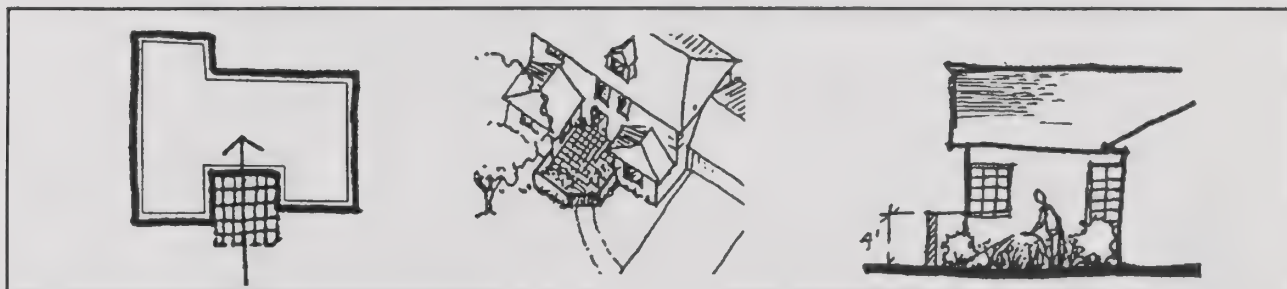
## 7.5 Entries & Facades

1. Building entrances should be emphasized through the use of lighting and landscaping.
2. Exterior stairways should be architecturally integrated into the design of the building. Thin, open metal, prefabricated stairs or railings are discouraged.



*Entries should be well defined*

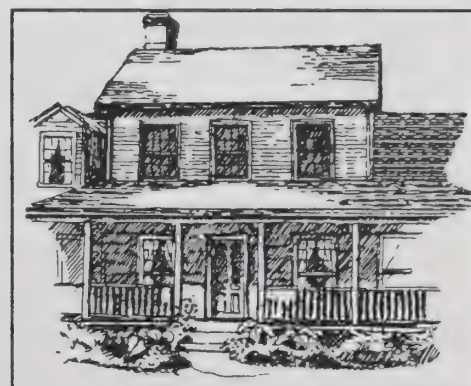
3. Each home should have a well-defined entry with careful roof and facade articulation to create individual interest and scale.
4. Contrasting colors may be used to accentuate building entry features and architectural details.



*Courtyard and building mass define the entry*

## 7.6 Porches, Stoops, Balconies, Courtyards & Patios

1. Porches, stoops, and balconies are encouraged to complement and enhance overall building design and function, and to vary building planes.
2. Covered porches or patios at the first floor level oriented toward the front and/or side yard should be a minimum of 6 feet in depth, and shapes, sizes, and setbacks should vary from house to house.
3. Victorian and bungalow style porches should include overhangs, heavy wood trellis structures or traditional lattice or picket surround courtyards whenever possible.
4. Architectural elements that add scale or interrupt the wall facade are encouraged, such as bay windows, courtyards, and porches.
5. The use of patios and courtyards are encouraged to provide private spaces. Varied paving textures and colors are encouraged.



*Porches, stoops, and balconies are encouraged*

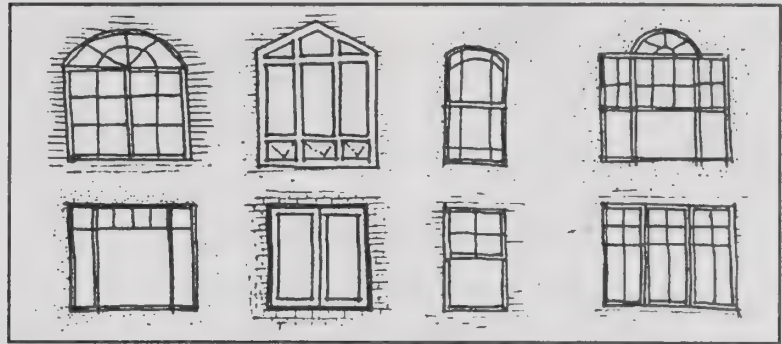




## 7.7 Windows

### 7.7.1 Proportion

1. All windows within a building should be related in operating type, proportion and trim. Unifying elements such as common sill or header lines are preferred.



Window examples

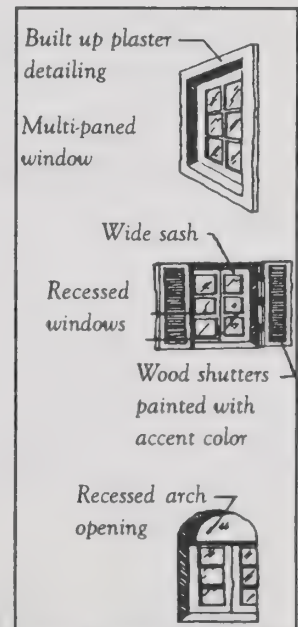
2. Where a larger amount of openings is desired, they should be designed as an aggregate of smaller, more intimate door and window openings.
3. Arrangement of building openings should be done with small, well-placed, and well-proportioned openings. Large picture windows without a multi-paned design are not acceptable.
4. Vertical rather than horizontal windows are encouraged.

### 7.7.2 Placement

1. Generally, windows should be placed a minimum of 12 inches away from the corner of the building, or the glazing on the intersecting wall planes should meet to form a corner window.
2. Built-up sills and trim should be used to create surface relief and texture.
3. Glass should be inset from the exterior wall surface and/or provided with dimensional trim to provide a sense of depth.
4. Window placement should consider privacy of adjacent residences.

### 7.7.3 Special Treatments

1. Black, bronze, silver, gold, or natural anodized aluminum and dark tinted or reflective windows are strongly discouraged.
2. True divided light glazing and external mullions are preferred over internal mullions. If true divided lights are not feasible due to cost, consider the following alternatives:
  - A. Plant-on external mullions may have the same visual effect at a lower cost. However, they have a tendency to break off over time. Periodic repairs may be required.
  - B. Windows with internal mullions (grids) vary in terms of appearance. Select windows that utilize thick, dimensional grids rather than thin strips.
3. Use of functional or decorative shutters is encouraged when appropriate to the architectural style.



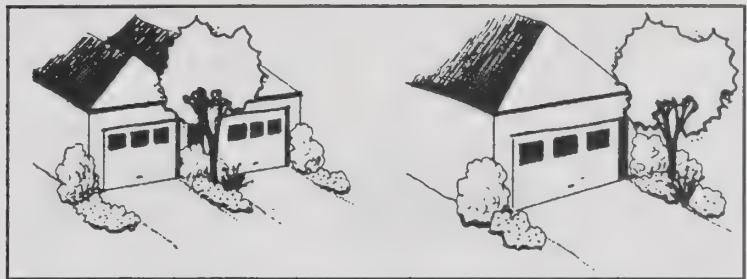
Special treatment of windows is encouraged



## 7.8 Doors

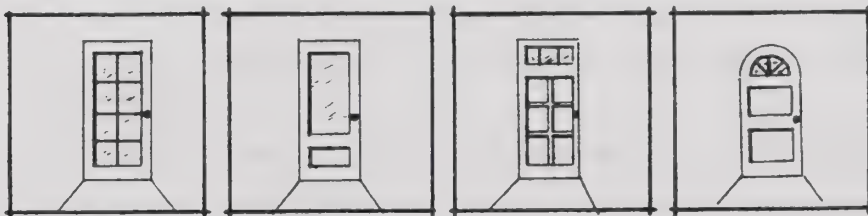
### 7.8.1 Garage

1. A garage accessible from an alley should be equipped with automatic garage door openers or set back at least 20 feet from the alley.



*Garage doors should be recessed into the garage wall and multi-paned*

2. Carports, detached garages and accessory structures should be architecturally integrated into the overall design of the project with similar materials and details as the residences.
3. The area allocated to garage space should be proportionate to the living area. Generally, the garage space should not occupy more than 25% of the total floor area.
4. No garage door or other opening providing access into a garage shall have a height of more than eight feet except with a Special Use and Development Permit approved pursuant to Chapter 6, Part 3 of the Land Use and Development Code.
5. Garage doors should be recessed into the garage wall and multi-paned with subtle adornment to provide shadow relief. Decorative panels are encouraged. Arched doorways or ornamental trim are encouraged.
6. Generally, metal roll-up doors should utilize embossed sectional panels, and the door should not be divided into more than four panels.
7. Use of zero-clearance garage doors is strongly encouraged.



*Multi-paned or french doors are encouraged*

### 7.8.2 Front/Primary Entrances

1. Doors can be wood or embossed metal, multi-paneled with subtle adornment to provide shadow relief. Decorative panels and/or windows are encouraged.
2. True divided light glazing and external mullions are preferred over internal mullions on doors when possible.

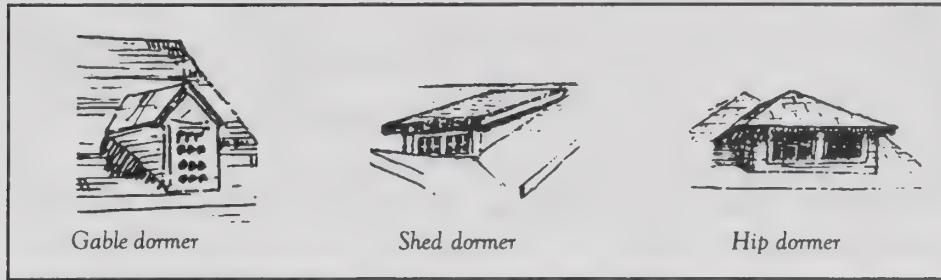
### 7.8.3 Secondary Entrances

1. Multi-paned or French doors are encouraged. Metal sliding glass doors are discouraged, unless a mullion effect is provided.



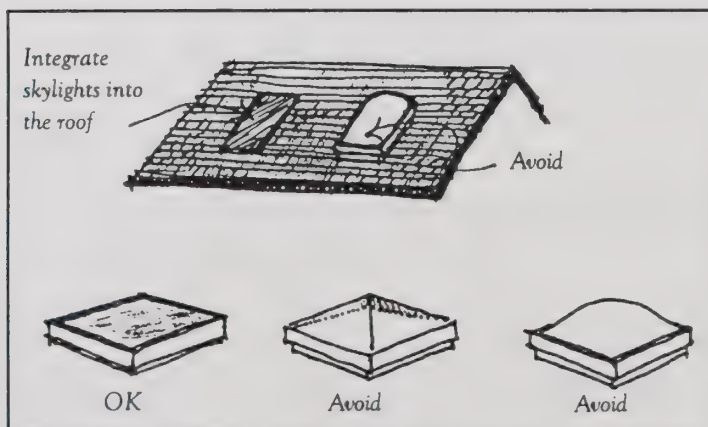


## 7.9 Roof & Chimney Styles

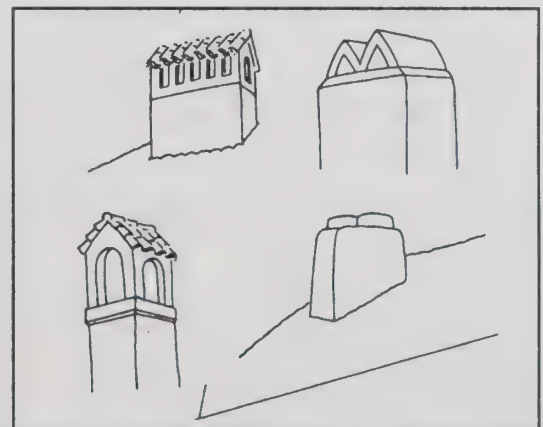


*Dormer variations are encouraged*

1. Roof forms should be consistent with the Village and neighborhood character. Dormers, eaves, exposed rafter tails, exposed roof beams, detailed corbels, turrets, eyebrows and triangular knee braces are common elements of the existing neighborhood that are encouraged in new designs.
2. Appropriate roof forms include gable, shed, and hip roofs. Roof pitch should be consistent with the architectural style of the building.
3. Avoid architectural elements such as A-frames, flat roofs and highly reflective surfaces. Short term trendy architectural styles are strongly discouraged.
4. Mansard roof forms should only be used when and if the building emulates a traditional style that employs mansard roofs (e.g., Victorian, Beaux Arts, etc.).
5. Roof articulations with dormers, cross-gables, and other architectural features are encouraged to minimize long, undifferentiated roof planes.
6. Skylights, if used, should be designed as an integral part of the roof with their form and color blending into the building. Flat skylights with clear or bronze glazing are encouraged, while bubble or dome skylights with frosted or colored glazing are not.
7. Articulated chimneys can add to the architectural character and are encouraged.

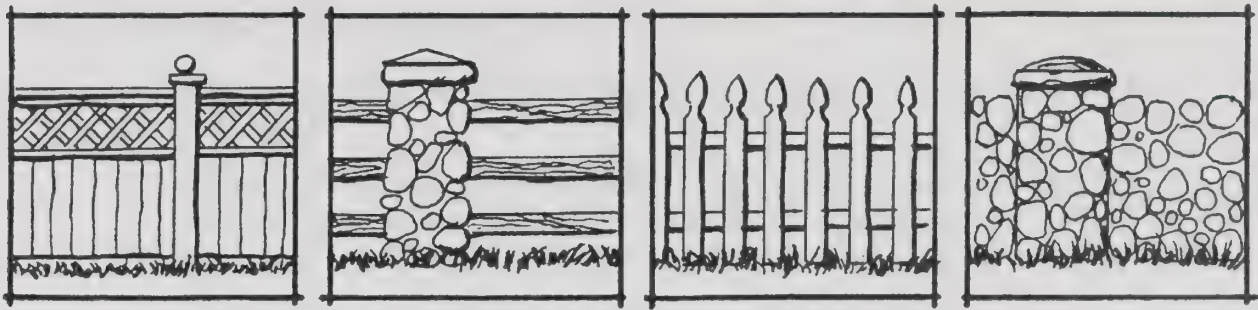


Skylights should be designed as an integral part of the roof



Examples of various chimney styles





*Fencing and wall materials should be used to reinforce the architectural theme of the building*

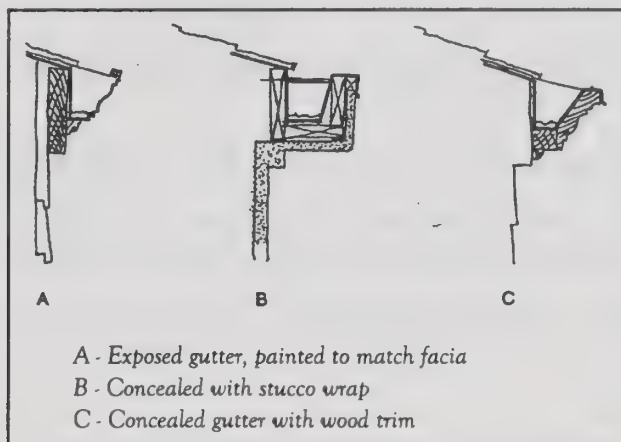
## 7.10 Fences, Garden Walls and Trellises

1. Fencing and wall materials should be compatible with the building and should be used to reinforce the architectural theme of the building. Stacked rock or battered base walls are appropriate, as well as, decorative wood fencing.
2. Use of indigenous and local historical materials for walls and fencing is encouraged. Walls and fences should incorporate wall caps and pilasters at entry points.
3. Perimeter walls, fencing and gates are subject to the height limitations of the Land Use and Development Code.
4. Front yard fences/walls/gates may not exceed 3-feet in height. These walls should not act as visual barriers in the front yard.
5. Fencing should encourage an open neighborhood atmosphere, while providing a definition between public and private spaces, through the use of low walls/fences/hedges around front yards.
6. Measures should be used to soften the building mass with architectural features such as garden walls, arbors, and trellises. However, it is important to avoid "tacking on" architectural features to hide poor massing and architecture.
7. Fence or wall colors should be compatible with the building.
8. Chain link, bare precision block or other concrete masonry unit (CMU) walls, and slumpstone are discouraged fencing materials.
9. Decorative iron or wooden gates to accentuate the fence, wall or building are encouraged.
10. Decorative and tile detailing is encouraged in such cases where column pilasters may be used.



## 7.11 Gutter and Downspouts

1. Gutters and downspouts should be concealed or designed as a deliberate architectural feature.
2. Exposed gutters and downspouts should be copper, have a factory finished metallic patina, or painted to match the surface they are attached to.



Gutters and downspouts should be concealed or designed as a deliberate architectural feature.





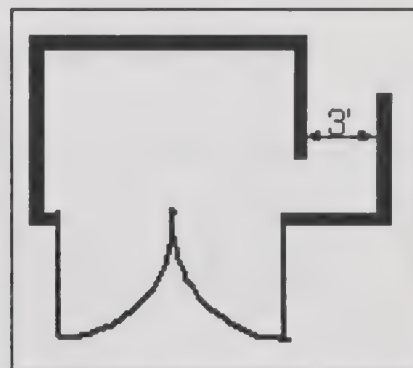
## 8.0 UTILITARIAN ASPECTS OF BUILDINGS

### 8.1 Screening Of Mechanical Equipment

1. Mechanical equipment (air conditioners, water softener tanks, solar collectors, duct work, meters, heaters, etc.), whether on the roof or the ground, should be screened from public view. The method of screening should be architecturally compatible in terms of materials, color, shape and size. The screening design should blend with the building design.
2. Utility equipment and meters should be accessible to the utility companies when screened.
3. All plans shall delineate the method of screening and the materials to be used.
4. Where possible, stacks, vents, antennas and other roof mounted equipment should be located away from public view on the least noticeable portion of the roof.
5. All flashing, sheet metal vents and pipe stacks should be painted to match the adjacent roof or wall material.

### 8.2 Trash and Recycling Storage Areas

1. Areas for centralized trash container storage and recycling bins serving multifamily projects should be incorporated into the building design and/or screened with walls and landscaping.
2. Trash and recycling storage areas should be located at the rear or interior side yards.
3. Refuse collection enclosures, where applicable, should be 4 to 6 feet in height and be constructed of durable, low maintenance and noncombustible materials.
4. Gates on refuse collection enclosures shall be self-closing, self-latching with interior opening mechanisms for escape outlets.
5. Centralized trash enclosures should be designed with a pedestrian opening to allow residents to access bins without opening the gates.



Centralized trash enclosure with pedestrian opening

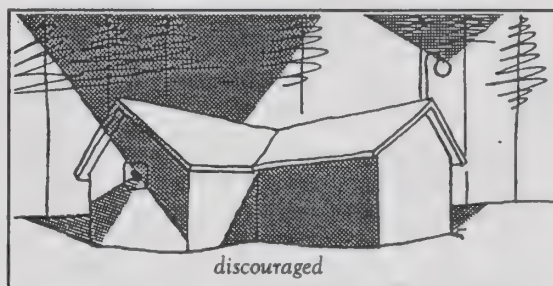
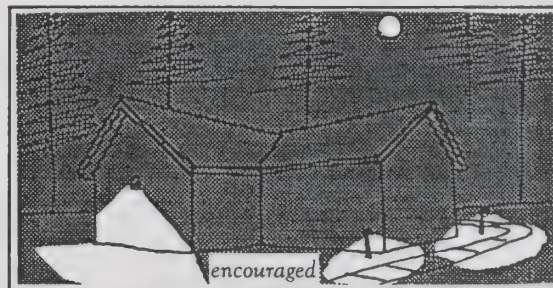
### 8.3 Mailboxes

1. Individual front-door service is encouraged. The use of gang mailboxes or rural posts is discouraged.



## 9.0 LIGHTING

1. All lighting is subject to the provisions and requirements outlined in Chapter 5, Part 3 of the Land Use and Development Code.
2. All exterior lighting shall be unobtrusive and designed to avoid glare into neighboring homes, public spaces, or into the night sky.
3. Lighting should be designed to confine the light within the site boundaries and to provide safety and security. All building entrances and pedestrian ways should be adequately lighted.
4. Light fixtures should be designed to be architecturally compatible and enrich the structures on a site.
5. Exposed bulbs are strongly discouraged.
6. The color, size and number of fixtures should be carefully considered and limited to functional needs.
7. Fixtures should be selected and arranged so that they direct illumination downward and not into the sky.
8. Security lighting where necessary should be hooded, recessed, or located in such a manner that lighting illuminates only the intended area, avoiding off-site glare or spilling over of unnecessary illumination.
9. No lights should be used on the top of any structure.
10. Height of a light pole should be an appropriate scale for the building or complex and site.
11. The use of low-voltage lighting is encouraged wherever possible.
12. Landscape light sources for wall washing and tree lighting should be concealed unless they are an integral part of the design element.
13. Flood lighting is not encouraged. Lighting should be used only to accent focal points and danger areas, not the entire yard or patio.
14. Color lights or colored light filters should not be used.



Lighting should be directed at a specific target





## 10.0 LANDSCAPING

Landscaping is an important part of the overall design of a project. The design should be diverse and creative and should be implemented with quality and resource-conservation in mind. The Architectural Commission of the City of Claremont encourages clear design concepts that fit within the surrounding community and complement adjacent buildings and parking areas. At the same time, designs should be original, incorporating well chosen plants, vibrant colors and textures. The effort to maintain the quality and ambience of the City of Claremont relies on well landscaped areas and the protection of established trees that make up Claremont's beautiful street scene.



*Natural, plentiful and mosaic plantings are encouraged*

### 10.1 Applicability

Landscape plans prepared in conjunction with new construction or major remodel projects will be reviewed by the City for consistency with the following guidelines. While subsequent residential re-landscaping projects are not typically subject to City design review approval, property owners are still encouraged to refer to the following criteria for design guidance.



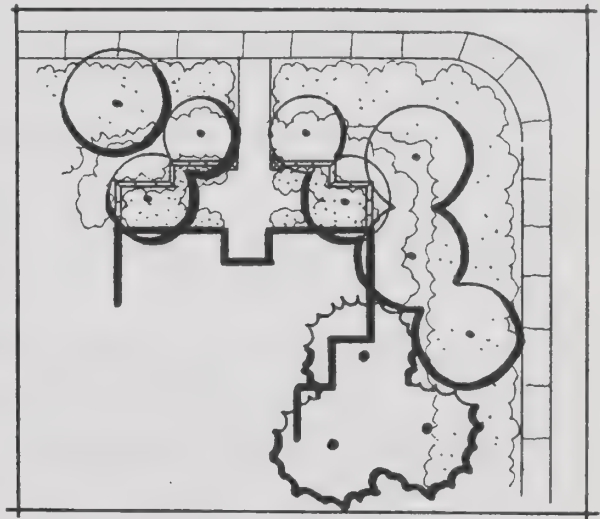
*Landscaping enhances the overall aesthetic appearance of a building.*

### 10.2 Landscape Design

1. Landscaping should be provided to enhance architectural features and entries of the structures.
2. Front and side yard landscaping are highly desirable and additional paving in those areas should be avoided when possible.
3. At least 50% of the front and street side setback areas are required to be landscaped.



4. Landscaping should be integrated into the overall site design for a project. Landscaping should enhance and complement the design of the building(s), preserve and enhance views, provide buffers, transition areas and screening.
5. Planting should be used to screen less desirable areas from public view, i.e., trash enclosures, parking areas, storage areas, satellite dishes, and public utilities.
6. Plantings should be designed as groups that have similar watering requirements.
7. Consideration should be given to plant species growth and size at maturity to limit overgrown landscaping.
8. Some commonly used planting design concepts include:
  - A. Specimen trees to provide rows at major focal points and entries.
  - B. Flowering vines on walls and arbors.
  - C. Pots, vases, window boxes and raised planters.
  - D. Trees to create canopy and shade, especially in parking areas and along pedestrian ways.
  - E. Flowering trees or seasonal flowers to provide color.
  - F. Berms, plantings and low walls to screen parking areas.
  - G. A hierarchy of plantings from grasses and ground covers to shrubs to trees.



*Front and side yards should be heavily planted*

### 10.3 Landscape Materials

1. Landscape materials that are appropriate to the local climate are encouraged.
2. The use of large specimen trees (24" box material, minimum) as specified in the suggested plan palette is encouraged.
3. Trees should respect the long-range views of the surrounding neighbors.
4. Planting areas should be covered with bark mulch at installation (approximately 3" thick) to increase the soil's ability to hold water and reduce water requirements as well as providing a clean finish to planting areas and aid in weed control.
5. Turf should not be used in areas narrower than approximately 3 feet.
6. Water-wise irrigation systems are strongly encouraged.
7. Use of native and low water use plants in conjunction with efficient and long-term water system for the establishment and maintenance of a drought-tolerant landscape is strongly recommended.



## 10.4 Suggested Plant List

The Community Service Department of the City of Claremont has prepared two approved plant lists (Village Plants and Plants Used in Claremont). The Architectural Commission has also adopted a Policy on Landscape Design. Landscaping materials should be selected in accordance with the lists and policy.

## 10.5 Landscape Structures

1. Use of landscape structures to provide an entry accent, shade, focal point, or gateway is encouraged. All landscape structures are subject to setback requirements as outlined in Chapter 4, Part 2 of the Land Use and Development Code.
2. Landscape structures should be an integral part of the building or accessory structure.
3. Landscape structures should be in appropriate scale within the site design.
4. Subtle garden art is encouraged.
5. Colors used to paint the landscape structures should be complimentary to the main building.
6. Placement of landscape structures should conform to all setback requirements and should consider visual impacts to and from the site.
7. Topiary plantings are discouraged. Shrubs and trees should be allowed to develop into their natural forms.

## 11.0 SIGNAGE – COMMERCIAL USES

All signage for commercial uses in the RMX district shall be provided in accordance with Section 446 of the Land Use and Development Code.





## 2.4.5 Commercial Mixed-Use Design Guidelines

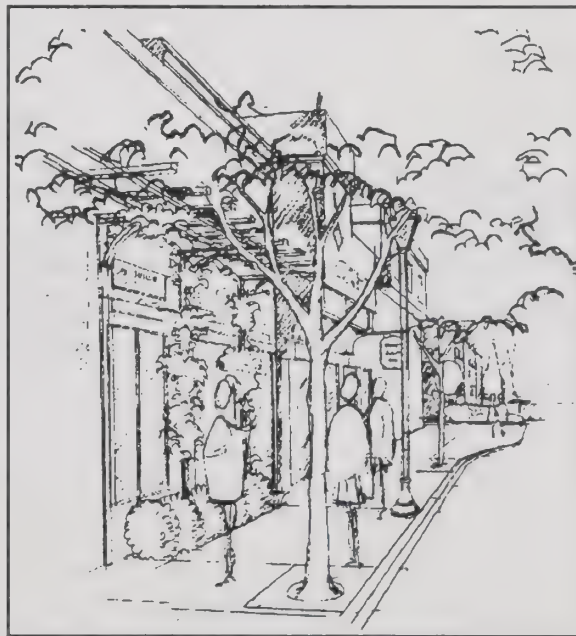
### Topics

2.4.5-1.0	Purpose
2.4.5-2.0	Applicability
2.4.5-3.0	Development Standards
2.4.5-4.0	General Design Principles
2.4.5-5.0	Site Planning and Design
2.4.5-6.0	Parking and Circulation
2.4.5-7.0	Architectural Design Guidelines
2.4.5-8.0	Utilitarian Aspects of Buildings
2.4.5-9.0	Lighting
2.4.5-10.0	Landscaping
2.4.5-11.0	Signage

### 1.0 PURPOSE

The Commercial Mixed Use (CMX) designation permits combining uses on a site which otherwise would not be allowed or required in a single zone. Further, the intent of the CMX zone is to create a pedestrian-oriented community and a reduction in auto travel through provision of services, entertainment opportunities, and housing in proximity to each other.

The goal of this section is to provide an established set of clear and usable design guidelines and criteria for creating an energetic and eclectic Village experience. These guidelines should assist the designer in developing an acceptable building design based on the existing scale and pedestrian character of the Village, which can be used to create future new buildings and renovate existing buildings. These guidelines are intended to be flexible and allow creativity and variation in the design of buildings that meet the operational business needs of individual property owners. Further, these guidelines are intended to create an overall pedestrian oriented streetscape for the core of the retail area of the Village.



*Pedestrian scale storefront overhang and buildings articulation reinforces village character.*



## 2.0 APPLICABILITY

The provisions of this section shall apply to all commercial development within the Village Expansion Area. Any addition, remodeling, relocation, or construction requiring a building permit within the CMX zone shall adhere to these guidelines. The design elements of each project, such as site layout, architecture, landscaping, signs, and parking design, will be reviewed on a comprehensive basis.

The City of Claremont may interpret these design guidelines with some flexibility in their application to specific projects. The guidelines will be utilized during the City's design review process to encourage the highest level of design quality, while at the same time providing the flexibility necessary to encourage creativity on the part of the project designers. The overall objective is to ensure that the intent and spirit of the guidelines are followed.

Unless there is a compelling reason, these guidelines shall be followed. Generally, the City will not waive a guideline unless the project designer can demonstrate that such a waiver will facilitate an innovative or otherwise preferable design concept.

The Architectural Commission may approve a design review application only if it determines that all of the Commission Review Criteria listed under Section 616 of the Land Use and Development Code (LUDC) are met. The Commission shall refer to these guidelines in making such determinations for development proposals in the CMX district.

## 3.0 DEVELOPMENT STANDARDS

Property development standards in the CMX zone shall be those of the existing CV-Claremont Village zone as outlined in the LUDC. The Claremont Village standards in the City's LUDC will be supplemented by the guidelines set forth in this section. However, use-permit or architectural review approval (via the Planning Commission and/or Architectural Commission) may include more provisions and standards to assure compatibility of uses and surroundings.





## 4.0 GENERAL DESIGN PRINCIPLES

### 4.1 Architectural Character

The intent of the CMX guidelines is to promote the continuation of the pedestrian-oriented characteristics of the Village. No single particular architectural theme is being promoted, but rather the emphasis is to promote variety with a respect for the existing historically important buildings and the pattern prevalent in the existing Village. Many of the styles and patterns shown in the following pictures and graphic illustrations represent a concept of recommended building elements and details as opposed to thematic images. Caution should be exercised when considering architectural styles that have recently become popular (i.e. “trendy”), but have not yet stood the test of time. Historic styles that cannot be faithfully replicated should be avoided.



*Architectural elements that are simple, but capture small town character are encouraged.*



*\* Image provided by the City of Claremont*

*New construction and renovation projects should respect the pedestrian oriented character of the Village.*



## 5.0 SITE PLANNING & DESIGN

Site planning refers to the arrangement of buildings and parking areas, the size and location of pedestrian spaces and landscaping, and how these features relate to one another. Site design addresses the scale and size of outdoor spaces, spaces between buildings and parking areas and the relationship of site elements that create a comfortable pedestrian environment.

Improved site planning in the Village Expansion area will increase pedestrian involvement and safety, create a traditional retail hub with strong street activity, edges and pedestrian arcades, while maintaining the “small town” feeling for Claremont residents and visitors.

1. Pedestrian linkages to nearby neighborhoods and other commercial property of close proximity should be provided whenever appropriate and feasible.
2. On larger sites, focal points should be developed to create a definite sense of identification. Plazas, landscape, fountains, artwork, textured pavement, universally accessible changes in pavement levels, and vertical building features may be combined to create focal points and identity.
3. Site plans should balance the need to provide adequate vehicular access, with the need to eliminate unnecessary driveway entrances and provide access points which are coordinated with other properties (particularly on Indian Hill Boulevard).
4. New development/ renovations/ additions are encouraged to incorporate public art consistent with the City’s public art program.



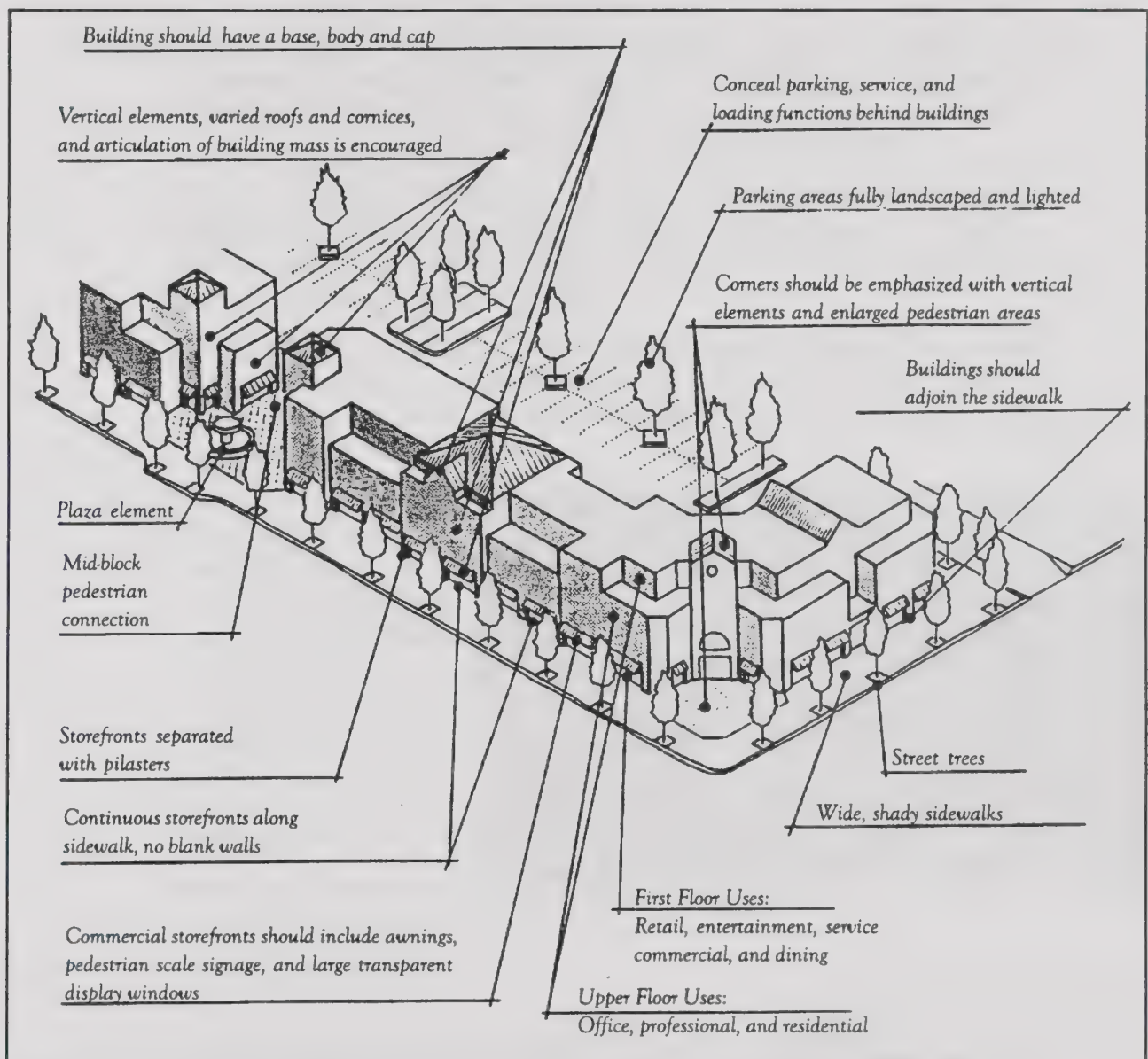
*Outdoor eating areas are encouraged.*



*Plaza with fountain, landscaping, and textured pavement creates a comfortable space.*







Good elements of site planning and building form massing contribute to the Village environment.

5. Each project is encouraged to have its own identity, yet any site development must integrate with adjacent properties to provide functional and aesthetically designed vehicular and pedestrian circulation.
6. Buildings should be sited and designed so that there are no barriers or other elements emphasizing property boundaries. Care should be taken when addressing the interface between two or more properties. Property lines should not be treated as walls and barriers.





7. Buildings should be oriented to create and define usable outdoor spaces. These spaces should have a microclimate that is responsive to the local weather conditions. A variety of local microclimate characteristics, such as shady, sunny, warm, cool, open, and private should be part of these spaces whenever possible.
8. Due to the high visibility of corner properties, extra care should be given to building orientation and articulation (see the Corner Building section). Significant buildings with prominent architectural features should be located near corners and intersections whenever possible.
9. Buildings should be sited close to, and oriented toward, the street. When possible, an arcade or lean-to canopy covering the sidewalk should be incorporated into the building storefront to allow for covered pedestrian walkways, outdoor seating, landscaping or shade.

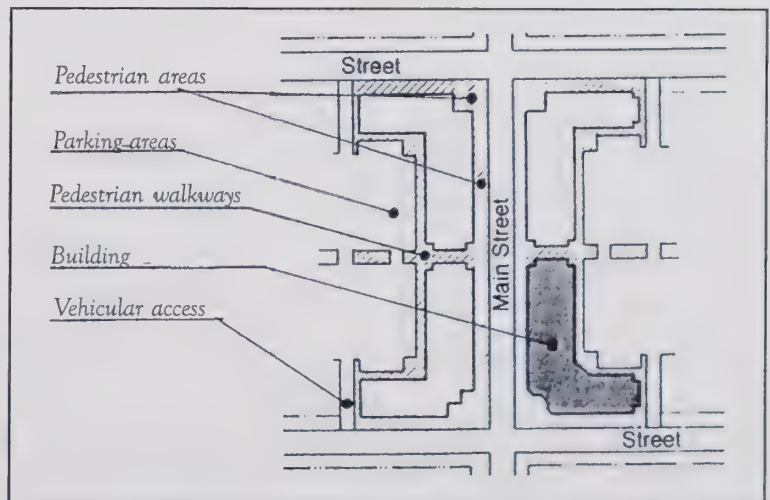
10. Portions of buildings may be set back from the street and alcoves may be provided to include small plazas, entry nooks, and outdoor cafe seating. A series of these spaces along an arcade provides an area for resting and permits areas for entertainment.



Buildings should be located at the street edge and include articulated storefronts, alcoves, and entry nooks.

11. Plazas and open spaces should be designed to be welcoming and inviting to people on the street.

11. Site furniture and light fixtures should reflect the same design concepts as the public right-of-way street furniture.



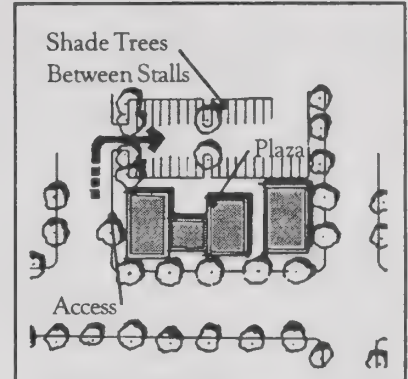
Preferred Building Siting: Shared parking behind the buildings, pedestrian access through buildings, & vehicular access located away from intersections.



## 6.0 PARKING AND CIRCULATION

This section is intended to ensure that parking lot designs are attractive and functional while at the same time meeting parking regulations.

1. Whenever possible new construction should accommodate inside the block surface parking lots with adequate directional signage at street edge.
2. Rear entrances to buildings should be provided where inner-block parking is provided.
3. Parking lots should provide areas for bicycle and motorcycle parking.
4. Driveways to parking lots should be located as far from street intersections as possible so that adequate stacking room along the street is maintained. A minimum distance of 100 feet is recommended.
5. Where possible, the entrance to parking lots should be located on side streets to maintain the pedestrian flow and avoid conflicts between vehicles and pedestrians on the main frontage. When this is not possible, design the front site entry with enhanced paving, landscaping, and architectural features to differentiate it from the sidewalk and make it a deliberate architectural feature.
6. Limit the number of curb cuts to public streets. Common driveways that provide access to more than one site are encouraged.
7. Parking stalls oriented at 90° generally provide the most efficient parking design. However, angled parking should be encouraged for large parking lots if it helps to accommodate more landscaping between rows of stalls and at the ends of rows.
8. Shared parking between adjacent businesses and/or developments is encouraged so long as parking for businesses does not adversely impact adjacent residential areas.
9. When walls are used to screen parking, breaks should be provided to allow pedestrian circulation, and the walls should be low enough for safety and security purposes.
10. Walls need to be finished and designed to compliment surrounding development.
11. Off-street parking facilities should be designed so that a vehicle does not have to enter a street to move from one location to any other location within the same parking lot.



*Parking should be located behind buildings and away from the street intersection.*





## 7.0 ARCHITECTURAL DESIGN GUIDELINES

### 7.1 Building Form

Building forms and facades influence cohesiveness, comfort, and aesthetic pride and at the same time can encourage shopping, increase a sense of security, and generate pedestrian activity. Where commercial buildings are neighbors to residential buildings or where infill buildings are being constructed, consideration of scale, detail and materials is very important. At the same time any good design must take into consideration some fundamental design principals including continuity, proportion, mass, scale and rhythm.

The following guidelines are intended to provide a general framework for design, and do not mandate specific architectural styles, themes or details. The City will be open to considering innovative, alternative design concepts that were not envisioned at the time that these guidelines were written, and may not fully comply with all of the criteria set forth below.

#### 7.1.1 Continuity

Continuity among individual buildings in the Claremont Village Expansion Area contributes to community identity, levels of pedestrian activity, and economic vitality.

1. As the Village Expansion Area develops, each subsequent building project should reflect the existing form, scale, and proportion (relationship of height and width) of the adjacent buildings that helped establish the design context for the area.
2. Infill buildings that are much wider than the existing facades must be broken down into a series of appropriate proportioned structural bays or components. Commercial street facade rhythm helps to tie the downtown streets together visually.
3. New buildings may be no more than one story higher or lower than their neighboring building.
4. Key facade elements on an existing building, (i.e. wall planes and modulation, window and door arrangements, entries, transoms, awnings and cornice treatments) may be repeated in the design of a new neighboring building.



*Reflect the existing form, scale, and proportion of adjacent buildings*



## 6.1 Parking Lot Area Planting

Landscaping within Village Expansion parking lots should be given special consideration. These areas are typically located out of the public right-of-way and may contain different planting materials than a neighboring street. To this end, these guidelines should provide a parking lot designer with the direction needed to create a functional environment that will blend with the Village areas.

1. Appropriate lighting and landscaping should be provided as per City standards, including shade trees and lamp posts based on the existing Village style (refer to lighting section of these Guidelines).
2. Landscaping should be provided at the rear of buildings next to parking areas.
3. A 5-foot minimum landscaping buffer should be provided between parking areas and public rights-of-way. The landscaped buffer area should not be included when calculating the minimum 5% landscaping requirement within the parking lot interior by the LUDC.
4. Trees should be located throughout a parking lot and not merely at the ends of parking rows. A minimum of one tree for every five parking spaces should be provided. Trees should be sized at 24-inch box or larger at the time of installation.
5. Vines, espaliers, and potted plants are excellent elements for providing wall, column, and post texture and color, as well as accentuating entry ways, courtyards and sidewalks.
6. Landscaping within parking areas should be protected from encroaching vehicles by concrete curbing or raised planting areas.
7. Trash enclosures and loading areas provided in the parking areas must be screened with landscaping or wall materials.
8. Trash enclosures should be separated from adjacent parking stalls by minimum 3-foot wide planters with low-growing plant materials to ensure that adequate space is available for passengers to access a vehicle in an adjacent parking space.
9. Consideration of plant materials adjacent to parking spaces should be a priority. Thorns, stickers, or sharp leaves should be avoided.
10. The height of landscaping adjacent to parking stalls is important to allow the opening of side doors and to allow for vehicle overhang.
11. Landscape islands should be a minimum of 5' in width to allow for tree growth and to avoid tree trunks from being hit.



*Landscaping defines parking areas*

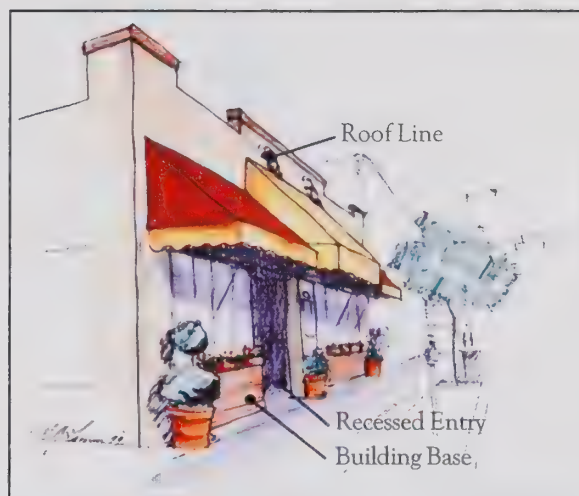




## 7.1.2 Mass

Mass is defined as a three-dimensional form such as a cube, box, cylinder, pyramid, and core. The way the forms are sized directly relates to the way building elements are emphasized or de-emphasized. Voids or open spaces in the forms can change their appearance and make the building more interesting and less imposing.

1. Each building should have a recognizable base, body, roof line, and entry.
2. Several smaller buildings rather than one large building should be used to provide an intimate scale.
3. Varying materials between base and body of a building can break long wall planes.
4. Changes in vertical planes break up a boxlike appearance. Vertical elements such as pilasters help create “bays” to give the appearance of several smaller buildings.
5. Spaces in commercial buildings should be designed so that they can be leased to more than one tenant.
6. Surface detailing should not serve as a substitute for distinctive massing.
7. Massing design may include:
  - A. Variation in the wall plane (project and recess)
  - B. Variation in wall height
  - C. Roofs located at different levels
8. New development should express its own uniqueness of location, tenant, or structure, designed especially for the particular building site and not as a copy of a generic building type which might be used anywhere.
9. The use of corporate “chain” architecture detracts from the unique character of the Village and is strongly discouraged. Corporate tenants will be required to design their buildings to fit the scale and character of the Village.
10. Upper floors may be appropriate for varied setbacks to accommodate balconies, seating and other architectural treatments.



*Each building should have a recognizable base, body, roof line, and entry.*



*Vertical elements break up long facades*



*Recessed entries with articulated storefronts should face pedestrian spaces and streets.*





11. Tall dominating structures should be broken up by creating horizontal emphasis through the use of trim, awnings, eaves, or other ornamentation, and by using a combination of complimentary colors.
12. Recessed entries and articulation in the storefront mass should be used.
13. Large areas of intense light color should be avoided.  
While more subdued colors usually work best for overall building color, bright trim or accent colors are appropriate for trim, windows, doors, and key architectural elements.

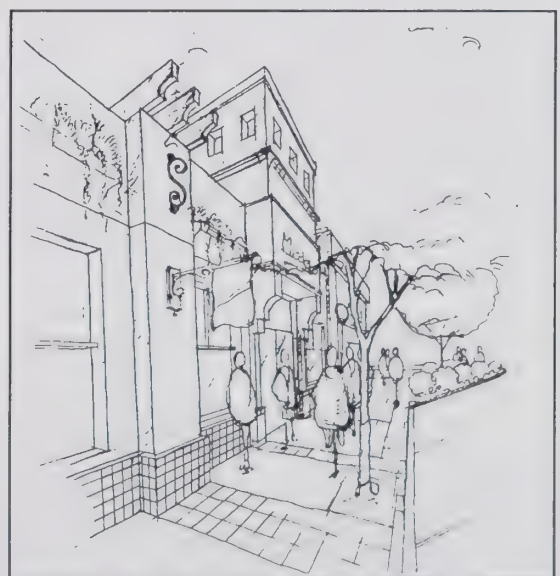
### 7.1.3 Scale

Scale is the proportion of one object to another. "Human" or "intimate" scale incorporates building and landscape elements that are modest in size. "Monumental" scale incorporates large or grand building elements. Buildings within the Claremont Village Expansion Area shall incorporate human scale elements.

1. Architectural details and materials on lower walls that relate to human scale such as arches, trellises, or awnings should be utilized.
2. Articulated storefronts with carefully arranged doors, windows, arches, trellises or awnings, rather than blank walls, should face onto pedestrian spaces and streets.
3. Projections and recesses are encouraged to add texture and differentiation between buildings. These elements could include bay windows or recessed storefronts that are large enough to accommodate seating for outdoor seating and/or to observe passers-by. Entry portals proportional to the buildings with columns and a detailed roof or balcony also add texture.



*Recessed entries and articulated storefronts are encouraged*



*Architectural details, materials and projections on lower walls and building surface create human scale*



### 7.1.4 Rhythm

Rhythm describes the relationship of building components, as well as the relationship of individual buildings to one another.

1. Rhythms should be more complex than simply the repetition of one or more architectural details.
2. Traditional horizontal rhythm intervals generally do not exceed 25 to 30 feet at the ground level, irrespective of a building's total width. Continuation of this familiar, human-scaled rhythm is encouraged in new construction.
3. Rhythm may be expressed by changing materials or color.
4. Rhythm may be expressed by using elements such as columns and pilasters.



*Building forms must possess rhythm, proportion, and balance.*

### 7.1.5 Proportion

The ratio of one dimension to another, such as the width of a building to its height defines its proportion. Unequal proportions can create horizontal or vertical emphasis.

1. Architectural proportions that have stood the test of time should be utilized.
2. Proportions that have historically been related to the selected architectural style should be utilized.



*Balance and proportion is well defined in windows, awnings, and rooflines.*

### 7.1.6 Balance

The visual equality between elements describes its balance. Harmonious visual impact is critical regardless if building elements are symmetrical or asymmetrical.

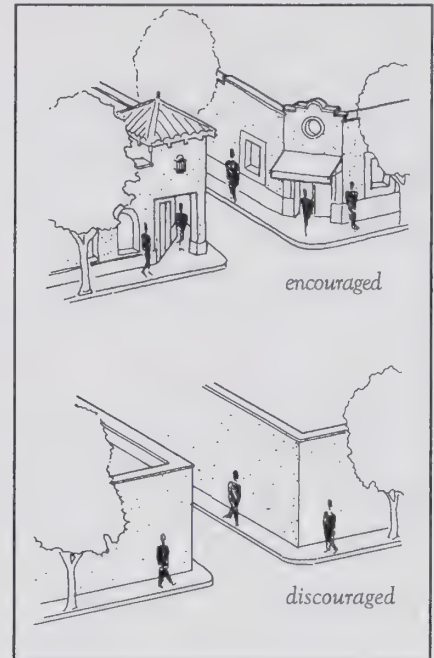
1. Building elements should be either symmetrical or asymmetrical.
2. All building elements should be consistent with the organization method selected. For example, a symmetrical element should not be juxtaposed into an asymmetrical design, or vice-versa.





## 7.2 Corner Buildings

1. Buildings located at key intersections should incorporate special architectural elements that create an emphasis of importance of that location. Such elements may include vertical projections, i.e., clock towers, diagonal walls at the corner, taller, prominent rooftop elements, and/or a substantial art form or fountain.
2. Parking lots are not permitted on corner sites.
3. Blank walls on corner building sides are discouraged.
4. Existing corner buildings with blank walls could add display windows, extend facade material, colors, and treatments to blank walls.
5. By the nature of its size, mass, and history, the packing house is a landmark building. (See Historic Site designation Section 2.4.7 for special provisions for the packing house site.)



Node buildings with special architectural elements at the corner are encouraged.

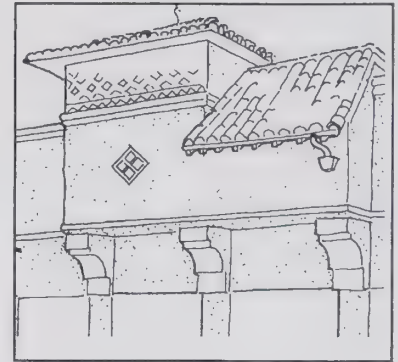


Tower identifies corner and street intersection.

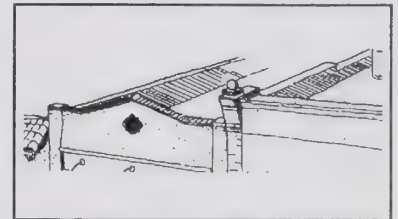


## 7.3 Roof Forms

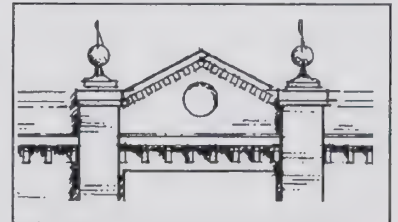
1. Multi-form roofs, gabled, and shed roof combinations are encouraged to create an interesting and varying roof form that will lessen the mass of the building, and add visual appeal.
2. Roof materials and colors are important aspects of the overall building design. They should be internally consistent with the desired architecture and complementary to adjacent structures. Refer to Section 7.8 Building Materials and Texture.
3. Parapets may be used when sufficiently detailed to screen roof mounted equipment and provide a contrast to gabled or mansard roofs.
4. Parapets should have sufficient articulation of detail such as precast treatments, continuous banding or projecting cornices, dentils, caps, corner details, or variety in pitch (sculpted).
5. Rooftop equipment on flat roofs shall be screened and not visible from ground level. Buildings with flat or low pitched roofs shall incorporate parapets, pitched facades, or architectural elements designed to screen roof mounted mechanical equipment and to be architecturally compatible with the design of the building facade.
6. Structural stability and the height of a parapet shall be coordinated with the placement of mechanical equipment so that equipment is not in a pedestrian's line of sight from street level.



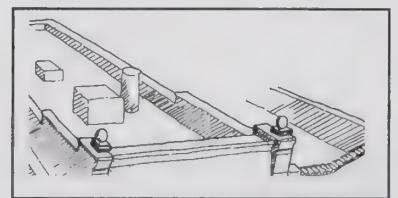
Using multiple roof forms create visual interest.



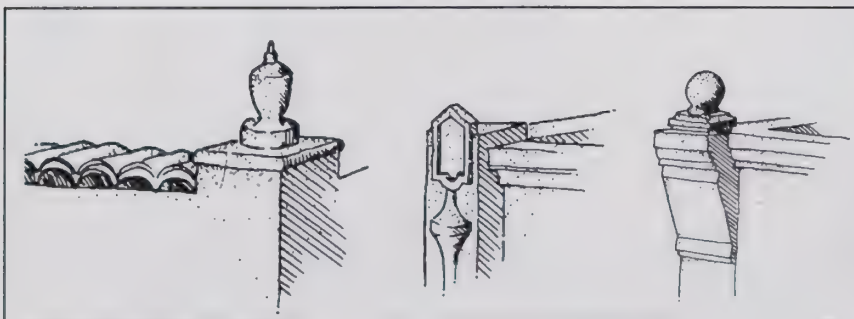
Parapets could be used to provide contrast.



Parapet articulation could include dentils, caps, and continuous banding.



Parapet screening of roof mounted mechanical equipment is preferred.



Corner details provide interest.



Ice House Office parapet evokes simple, classic design.





## 7.4 Entries and Doorways

It is important that the main entrance to a building is clearly identifiable and unique. It is the primary point of arrival and should be treated as such.

One or more of the following methods should be incorporated in the entrance design.

1. A change in wall / window plane or wall articulations around the door projecting beyond the door.
2. Placement of art or decorative detailing at the entry.
3. A projecting element above the entrance.
4. A change in material or detailing.
5. Implementation of architectural elements such as flanked columns or decorative fixtures.
6. Recessed doors, archways, or cased openings.
7. A portico or formal porch projecting from or set into the surface.
8. Changes in the roofline, a tower, or a break in the surface to the subject wall.
9. Upper floor entries at the street frontage should have their own distinct design that compliments the main building frontage.



*Entry is clearly articulated.*



*Additional architectural detail and a change in the roofline emphasizes the entry.*



*Angled entry identifies primary access point.*



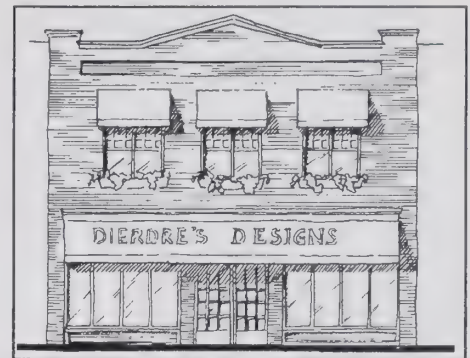
*Recessed entry allows for outdoor seating*





## 7.5 Windows and Doors

1. At the street level, windows should have pedestrian scale and detail. The framing provides opportunity for color variation and detail.
2. Clear glass is recommended on street level to create interesting interior shop views for pedestrians. Heat gain can be limited by implementing awnings, recessed storefronts, polarized glass, or professionally applied UV film. Reflective, mirrored or tinted glass above 70%, is strongly discouraged.
3. Storefront windows and doors should be of the same style. The line established by uniform storefront heights helps to establish a sense of scale for pedestrians.
4. Windows on upper floors should reinforce the typical rhythm of upper story windows found on traditional commercial buildings.
5. The first floor of commercial buildings should be predominately transparent to help distinguish it as a more publicly oriented space. Windows, when combined with entry doors, should occupy 50% to 70% of a first-floor frontage. The second- and third-story windows should make up a minimum of 30-50% of the wall area.
6. Windows and doors should be in scale with the building elevation on which they appear. Recessed openings, windows and doors provide depth and can help break up the apparent mass of a large wall.
7. Windows may be combined with wall planters at their base or use a base material that has color and texture such as ceramic tiles, brick, river rock, or raised wood panels.
8. Operable windows are encouraged to provide natural air circulation and allow interchange between indoor and outdoor spaces.
9. Well-designed storefronts, including windows, doors, wall composition, colors, and materials are very important to create a sense of entry and pedestrian scale.
10. Doors, windows, and openings should be used to add extra texture to the wall plane.



*Windows on the second floor represent the typical rhythm found on traditional commercial buildings.*



*Windows above awnings provide additional interior light.*



*Recessed storefront with tile creates a sense of entry and pedestrian scale.*



## 7.6 Awnings and Umbrellas

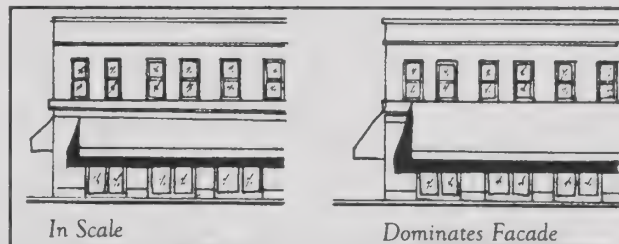
1. Awnings add color, forms, relief, and pedestrian protection from the elements. They may also create a space for identification signage.
2. Awnings, as well as signage, help to clearly demarcate building entries and help orient pedestrians.
3. Awning maintenance should be in accordance with the awning manufacturer's care instructions. The life of the awning is generally not expected to exceed eight to ten years. Property owners should not propose installing awnings unless they are prepared to replace the awnings every eight to ten years.
4. Awnings with imprinted signage are encouraged. Lettering style and size should be in scale with the awning and adjacent awnings subject to the existing sign ordinance.
5. Awnings and umbrellas should be made of cloth (not plastic or vinyl) and should be high enough so as not to inhibit pedestrians.
6. Awnings should not be wrapped around buildings in continuous bands. Awnings should only be placed on top of doors, windows, and other openings where arcades are not utilized.
7. Awnings should not dominate the facade, but should be in scale with the rest of the building.
8. Awnings and umbrellas should be regularly maintained and kept free from tears, fading, and stains.
9. Awnings, if lighted, should be lit with direct, architecturally interesting and appropriate fixtures such as goosenecks.
10. Awnings should be placed within vertical elements when the facade of a building is divided into distinct structural bays.



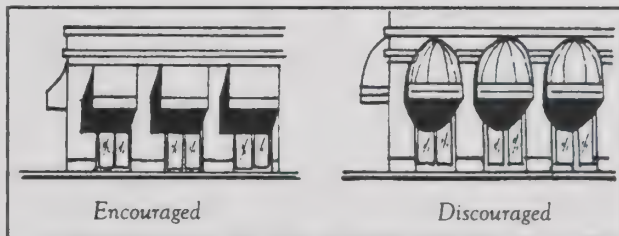
Awnings add color, forms, and relief to a facade.



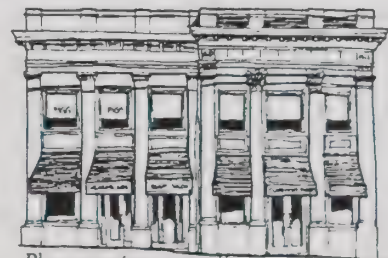
Awnings create pedestrian protection and scale.



Awning in scale with facade.



Shed awning consistent with rectilinear building form.



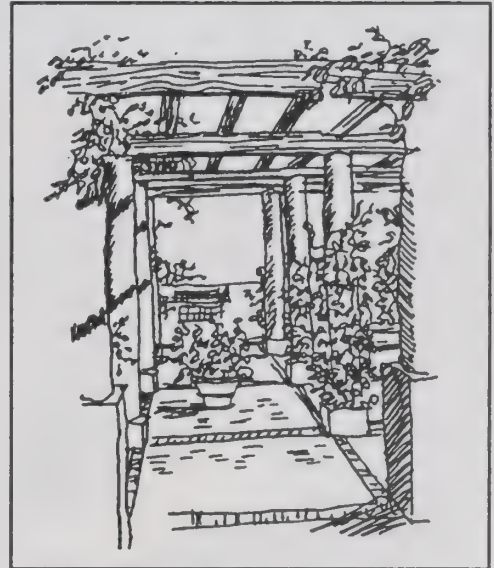
Place awnings according to structural bays.



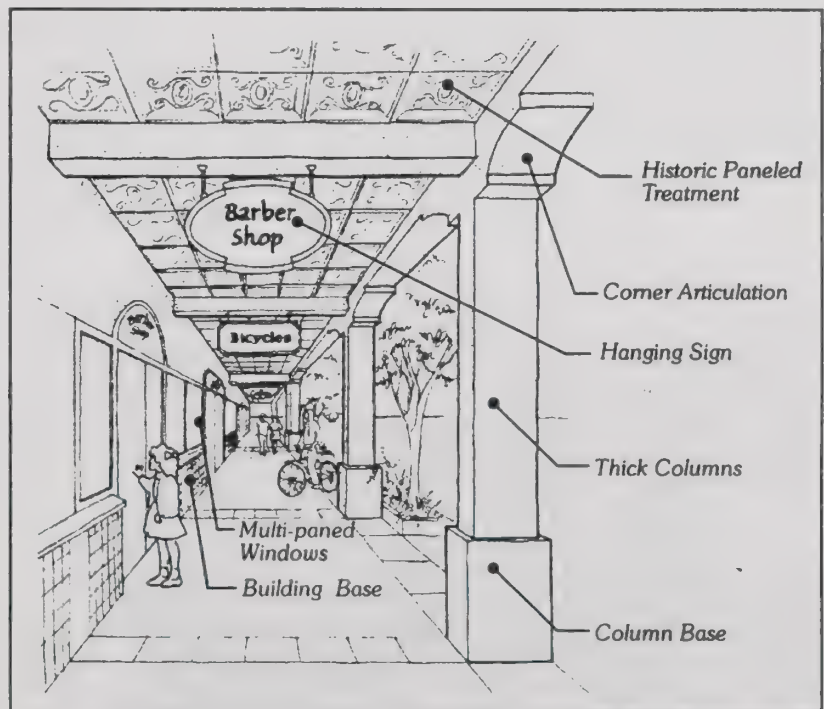


## 7.7 Arches, Porches and Covered Walkways

1. Buildings that contain multiple tenants should utilize pedestrian connection elements, i.e. arcades and internal courtyards.
2. Covered walkways provide a visual, as well as protective linkage between uses. These walkways may occur at building street frontages, between buildings, from buildings to parking lots, and within a parking lot.
3. Covered walkways associated with buildings should utilize the materials of that building. Freestanding walkway covers should have a similar design made with similar materials to be easily recognized throughout the plan area. Suggested materials include finished metal framing combined with decorative screens as the "canopy" or wood framing with living materials.
4. Archways and columns may also be used to accent store entries, courtyard entries, or as corner elements at key intersections. Materials used should complement the building(s) it is associated with, or the materials reflective of an important historic period.
5. Walkways that are "enclosed" by buildings provide an opportunity for pedestrian scale wall treatments such as murals, alcoves, or vines.



Wood framing trellis with living materials creates a pleasant walkway.



Covered walkway preferred details.



## 7.8 Building Materials and Texture

New buildings adjacent to historic or existing buildings in the commercial district should use materials that are similar to or compliment the historic buildings, but should not contrast with them. The selection and placement of building materials should provide visual interest at the pedestrian level. Heavier materials should be used to form the building base and as accents on upper stories and walls. Architectural details should be used to enhance the buildings and adjacent pedestrian spaces by adding color, shadows, and interesting forms. In addition, special consideration will be given to the interpretive use of materials found on vintage industrial buildings given the industrial history of the Village Expansion Area.



*Example of appropriate use of local materials.*

### 7.8.1 Exterior Materials

The following listings of appropriate and inappropriate materials were developed based upon the professional judgment and opinions of those involved in the development of these guidelines. The materials listed as “appropriate” have generally stood the test of time to have desirable aesthetic qualities, while those listed as “inappropriate” have failed to do so. Certain materials have been omitted from either column because their success or failure has more to do with the overall design, rather than the materials themselves.

These listings are intended to provide general guidance, and are not intended to be exhaustive; designers should not feel that they must limit their material palette to the following selections. New materials and refined fabrication methods are continually being introduced (some of which may allow some materials to be dropped from the “inappropriate” column), and Claremont encourages designers to take advantage of these innovations.



*Example of incorporating industrial form and materials in commercial design.*





### Appropriate Primary Wall Surfaces

- Natural rock
- River rock
- Unpolished marble
- Unpolished granite
- Clear glass with wood or quality metal trim
- Float finished stucco
- Smooth sand finished hand troweled stucco
- Hand troweled plaster surfaces
- Tile
- Clay brick
- Precast concrete
- Concrete

### Inappropriate Primary Wall Surfaces

- Simulated materials such as cement brick
- Rough hewn lumber
- T-111 Siding
- Slump stone
- Split face block
- Highly polished materials
- Spray on "Spanish lace" or knock down stucco
- Cultured stone
- Mirror glass and heavily tinted glass
- Vinyl and aluminum siding
- False stone veneer
- Corrugated fiberglass
- Chrome
- Highly polished materials

### Appropriate Roofing Materials

- Clay tiles
- Wood shingles / shakes
- Slate
- Seamed or ribbed metal
- Stamped metal
- Copper

### Inappropriate Roofing Materials

- Concrete units that imitate wood shake, slate and clay tiles
- Fiber cement units that imitate slate or wood shakes
- Tile units that imitate wood shakes, shingles or slate
- Maxitile

### Appropriate Trim, Accent and Details

- Natural rock
- River rock
- Unpolished marble
- Unpolished granite
- Clay brick
- Heavy wood beams
- Ornamental tile for accent
- Prefabricated molding forms
- Inlaid tile
- Natural stone
- Wrought iron
- Terra cotta
- Milled wood
- Chrome
- Stainless Steel
- Highly polished materials

### Inappropriate Trim, Accent and Details

- Simulated materials such as cement brick
- Rough hewn lumber
- Cultured stone
- False stone veneer
- Corrugated fiberglass
- Plastic





## 7.9 Colors

Claremont encourages variety and artfulness in the selection of exterior colors for commercial buildings in the Village Expansion Area, and does not wish to place any pre-emptive restrictions on color choices. The following guidelines intended to promote well-coordinated color palettes that integrate with the other exterior features of a building.

1. Buildings should keep a balanced color palette between base colors and “brighter” or “darker” accent colors on each building.
2. Colors should be used that reduce sun glare on wall planes by using flatter, muted colors, i.e. avoiding bright whites.
3. Door and window trims, awnings, and wall tiles provide opportunity for color that adds interest and texture to storefronts or building bases. Color of trim should be coordinated with the wall colors.
4. Colors should be coordinated with other adjacent buildings.
5. Colors should coordinate with natural / unpainted materials used on the facades such as river rock, pressure treated wood, terra cotta, tile, brick, and stone.

## 7.10 Sides and Backs of Buildings

1. Murals, trellises, or vines and espaliers should be placed on large expanses of walls at the rear or sides of buildings to soften and create interest.
2. Marquis display cases may be provided between buildings in pedestrian linkage areas. Such display cases may include theater movie posters, upcoming civic events, retail events (such as sidewalk sales, book signing, etc.), art displays or shows.
3. Wall mounted lighting should be provided between buildings to ensure security.
4. Restaurants and cafes should use side of buildings for outdoor seating.



*Seating is encouraged between buildings, making dormant spaces active and inviting.*



## 8.0 UTILITARIAN ASPECTS OF THE BUILDINGS

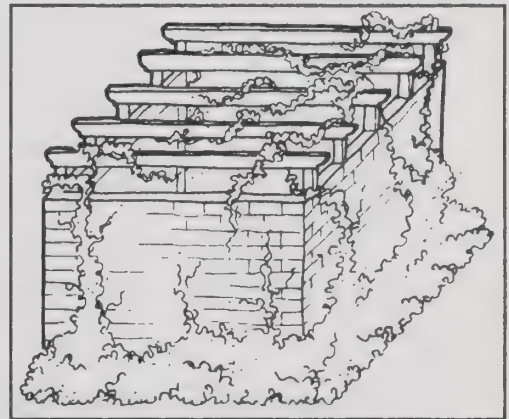
Utility service areas should be part of the early building design process, rather than an after-thought at the construction document phase.

### 8.1 Screening of Utilitarian Equipment

1. Service, utility, and loading areas should be carefully designed, located, and integrated into the site plan. These critical functional elements should not detract from the public viewshed area.
2. Siting of noise and odor generating functions on any CMX zoned site that may create a nuisance for the adjacent properties should be avoided.
3. Roof access should be provided from the interior of the building. Exterior roof access ladders are inappropriate.

### 8.2 Trash Enclosures

1. Every property should provide a trash enclosure that is capable of handling the refuse generated by that site.
2. A pedestrian entrance to the trash enclosure should be provided so that the large access gates do not have to be opened as often.
3. Recycling bins should be integrated into the enclosure.



Trash enclosures shall be carefully designed and screened

### 8.3 Disabled-Accessible Ramps and Railing

1. Ramps and guardrails used as a means of egress must conform to the criteria listed in Section 1003.3.4 of the Uniform Building Code.
2. Guardrails should compliment the architectural style of the building while conforming to the UBC.
3. Ramps can be integrated into the site design creating functional or unique spaces.

### 8.4 Exterior Utility Panels

1. Utility service areas, such as electrical panels, should be placed within enclosures that are architecturally integrated into the building design.





## 8.5 Fire Apparatus

1. Consideration should be given to the design of new and remodeled structures to incorporate easy access to fire apparatuses which should be installed per the Uniform Fire Code.
2. Access for fire apparatus should be part of the planning process so as not to disrupt the visual integrity of a project.

## 8.6 Roof Drainage

1. Gutters and downspouts on the exterior of the building should be decorative or designed to integrate with the building facade.
2. Roof scuppers should not be used in areas that are visible to the street or public spaces.

## 8.7 Mailboxes

1. Common mailboxes should be located close to the project entry and the design of the box enclosure should be similar in form, materials, and color to the surrounding buildings. Mailbox locations must be approved by the U.S. Postal Service).
2. Mailboxes should be located in alcoves away from the streetscape.

## 8.8 Food Service

1. Grease traps should be designed and implemented per the Uniform Plumbing Code.
2. A plan for grease removal / extraction from the facility should be created. If grease barrels are to be stored on site for eventual removal then they need to be either accommodated and secured within the trash enclosure or in another approved location.
3. Grease barrels with spill containment basins are recommended.
4. Food businesses must demonstrate compliance with the National Pollution Discharge Elimination Standards in order to prevent outdoor washing of mats, equipment, etc.



## 9.0 LIGHTING

1. Effective lighting provides safety and direction for vehicles and pedestrians, visibility and security for businesses, while enhancing architectural building and landscape details. These guidelines apply to on site lighting for private development projects in parking areas and lights associated with the building. Light types could include pole lights, spotlighting, wall mounted sconces, parking and landscape lighting. Refer to the Land Use and Development Code, Chapter 5, Part 3, for lighting regulations.
2. An illumination plan and photometric analysis shall be submitted with architectural and site plans for review and approval. This plan shall provide the luminary data for each light source. The purpose is to ensure that adequate lighting is provided without excessive overlap of illuminated areas. Sensitivity to the mix of residential / commercial uses should be considered in choosing light sources and footcandle power. Lighting should be designed to provide ambiance, safety, and security without unnecessary spill over or glare onto adjacent properties. This is particularly important for the residential users who may be located on a second or third floor above a commercial use.
3. The quality of light, level of light as measured in footcandles, and the type of bulb or source should be carefully addressed. Lighting levels should not be so intense as to draw attention to the glow or glare of the project site.
4. Spotlighting or glare from any site lighting should be shielded from adjacent properties and directed at a specific object or target area. Exposed bulbs should not be used. Cut-off lighting is preferred.
5. The major source of lighting should be from street lights and window display areas.
6. Light fixtures should be designed or selected to be architecturally compatible with the main structure and the theme of the Village.
7. Uplighting of building elements and trees are effective and attractive lighting techniques that are strongly encouraged.
8. Low-voltage lighting conserves energy and must be used in the landscape whenever possible.
9. Colored lights are prohibited.
10. Height of a light pole should be appropriate in scale for the building or complex and the surrounding area. Pedestrian light poles along sidewalks or pathways and parking lot light standards should be 10' to 15' high.
11. Landscape lighting can be used to accent walkways and entries and/or seating areas and specimen plants / trees.



12. Indirect window display lighting is encouraged to create an interesting and friendly atmosphere. Interior store lights should be left on after the store has closed.
13. Walkways/paseos should be lit to an average 1.5 to 2-foot candle intensity to ensure safe nighttime conditions.
14. Landscape lighting is encouraged, but it should follow these guidelines:
  - A. Quality and well directed light is the goal. Too many light fixtures should not be specified.
  - B. Light sources for wall washing and tree lighting shall be hidden.





## 10.0 LANDSCAPE / HARDSCAPE

On the whole, the city's existing lush landscaping, with a variety of shade trees and colorful understory, will continue into the expansion area. All privately landscaped areas should strive to follow this same planting arrangements and careful maintenance practices.

### 10.1 Plant Materials and Layout

1. The Community Service Department of the City of Claremont has prepared two approved plant lists (Village Plants and Plants Used in Claremont) as well as an Architectural Commission Policy on Landscape Design. Landscaping materials should be selected in accordance with the lists and policy.



*Landscaped bulb-outs soften street edge*

2. Use of native and low water use plants in conjunction with efficient and long-term water system for the establishment and maintenance of a drought-tolerant landscape is strongly recommended.
3. Artwork and sculpture are encouraged to be incorporated into any landscape design.
4. Use of window boxes for upper story users is encouraged to provide color-spots at higher elevations, but plants must be accessible for maintenance and should be attached safely and securely.



*A mosaic divan is an example of incorporating artwork and sculpture in landscaping design.*

5. Plant material such as evergreens should be used to soften structural edges. It may not be used as a mask to justify poor building design.
6. Water features should be used with planting and natural materials in courtyards and plazas.



7. Trees and shrubs should be located and spaced to allow for mature and long-term growth. Trees and shrubs should provide minimal root problems.
8. Planters should be planted primarily with native plants that are low maintenance.
9. Accent planting should be used around entries and key activity hubs.
10. Formal planting designs and color-spots are encouraged in courtyards, plazas and in tree wells along the street frontages.
11. Trees should be used to create an intimate scale, enclose spaces, and frame views, but their placement must respect the long-range views of surrounding neighbors.
12. Planting should be used to screen less desirable areas from public view, i.e., trash enclosures, parking areas, storage areas, loading areas, and public utilities.
13. Consideration of seasonal shading from trees and shrubs should be considered when developing planting schemes for courtyards and streetscapes.
14. The irrigation systems shall be designed to apply water slowly allowing plants to be deep watered and reducing runoff. Drip systems are required in all areas except turf irrigation and small ornamental planting.
15. Deciduous trees provide solar control during summer and winter while providing fall color, seasonal flower and other desired effects.
16. Plants to be avoided include: short lived plants and those susceptible to disease; shrubs and ground cover that have become commonplace in Southern California; and large expanses of single plant varieties due to their unchanging appearance and loss of landscaping once struck with disease.
17. Lawn areas should be limited to a maximum of 1/3 the area devoted to plant material and should to be used only to serve a functional purpose.
18. Pruning should be done in order to preserve the health, structural integrity, beauty, and longevity of the plant. It should not be used to clip plants into unnatural geometric shapes. Refer to the Architectural Commissions Policy on Landscape Design for further guidance on pruning.
19. The City of Claremont requires that all properties be well maintained in order to have positive effects on the adjacent area and on the community as a whole. Dead plants should be replaced as soon as possible to preserve the appearance of the property.





## 10.2 Paving Treatment

1. Those areas between privately-owned properties and the street right-of-way may be paved with a different material than the sidewalk to accentuate entry ways or other pedestrian ways.
2. Courtyards, outdoor patios, arcades, and plazas provide inviting open spaces. These spaces should have detailed and well-defined paving design. Materials may include concrete or brick pavers, tile, scored, colored, and textured concrete. These spaces may be provided adjacent to building entries or facades at intersections, mid-block between buildings, and adjacent to parks.
3. Durable, smooth and even surfaces should be used in well-traveled areas while less traveled areas could utilize other materials which are appropriate for minimal use.
4. Patterns and colors should be installed in paving treatments using tile, brick, textured concrete in order to provide clear identification of pedestrian access points into buildings, parking features (i.e., handicap spaces, pedestrian loading, bus stops/pull-outs, etc.)
5. Tile or metal inlays in paving areas are encouraged for artistic interest and may possibly serve as public art or can be functional art as a time piece, directional marker, or historical anecdote.



*Example of material used in a less traveled areas.*

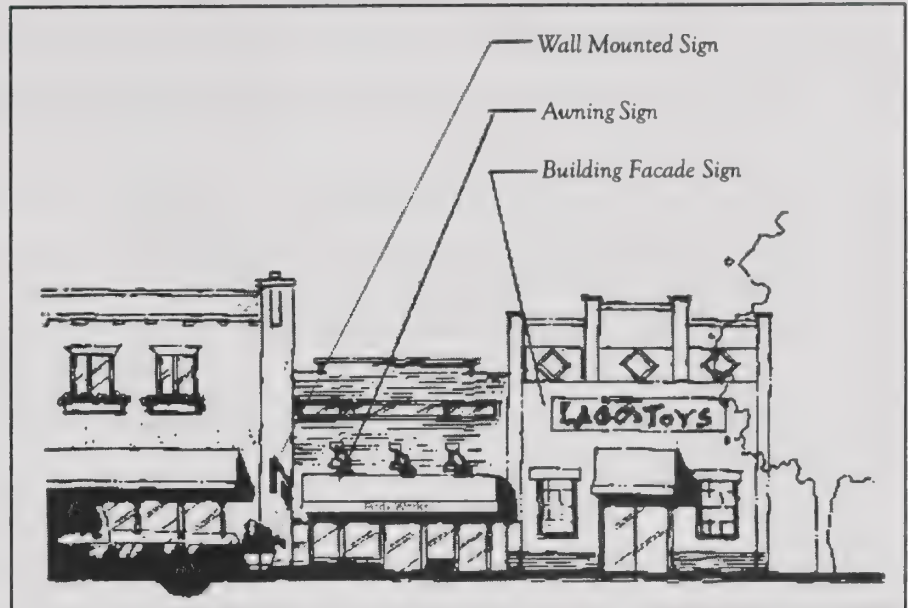


*Tile inlays add artistic interest and provide a smooth and even surface for well traveled areas.*



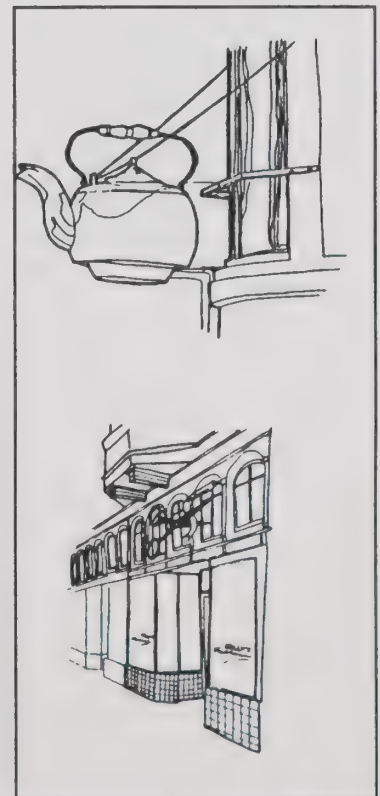
# 11.0 SIGNAGE

1. A sign program shall be submitted with design review application for new buildings. Regulations for sign dimensioning and area limitations are provided in the Claremont Land Use and Development Code.



*Typical sign types*

2. Signs reflecting the type of business through design, shape, or graphic form are encouraged.
3. The method of sign attachment to the building should be integrated into the overall sign design chosen.
4. Signs should coordinate with the building design, materials, color, size, and placement.
5. Signs should not cover up windows or important architectural features.
6. Sign variety is encouraged among different users.
7. Merchandise displays in storefronts should be altered or changed on a periodic basis. Storefront displays should maintain "permeability" into the shop interiors.



*Signs reflecting the type of business through design, shape, or graphic form are encouraged.*



8. Flush mounted signs should be positioned within architectural features, such as the panel above the storefront on the transom or flanking doorways.
9. Flush mounted signs should align with others on the block so as to maintain the existing pattern.
10. Internally-illuminated sign cabinets are strongly discouraged.
11. Hanging signs attached to buildings that project perpendicular to the building should be a minimum of 8' from ground level to the bottom of the sign. Signs that project should be small and reflect the use of the business by incorporating symbols or logos of the business.
12. Historically significant buildings should use signs that are typical to the era of that building.
13. Signs incorporated on awnings should occur only on the valance.
14. A single development with multiple users should provide a unifying sign theme.
15. Lighting of all exterior signs should be directional to illuminate the sign without producing glare on pedestrians, autos, or adjacent residential units.
16. Neon business identification signs, if designed tastefully, can add drama and character to a storefront. They may be used to reflect the style of the building and type of business and are subject to Architectural Commission approval.
17. Signs at storefronts may include permanent painted window signs that do not obstruct the pedestrian views into the store. Handcrafted wall-mounted signs or individual letters may be used also. All window signage, whether permanent or temporary, is subject to the limitations set forth in the LUDC.



Painted window sign





## 2.4.6 Mixed-Use Design Guidelines

### Topics

#### 2.4.6-1.0 Purpose

### 1.0 PURPOSE

The MX designation permits combined uses in a site which would otherwise not be allowed or required in a single zone. This zone encourages the same design character as CMX but expands allowable uses to allow for residential, visitor serving and office uses as the primary use, depending upon market demand.

The applicable zoning category, development standards and design guidelines shall be driven by the primary use at the time of development application submittal.

Generally, if the primary use is residential, then the R / RMX Design Guidelines apply. If the primary use is visitor serving, commercial or office, the CMX Design Guidelines apply. Determination of the primary use shall be made by the Planning Director.



## 2.4.7 Packing House Re-use Criteria

### Topics

2.4.7-1.0 History

2.4.7-2.0 Re-Use Criteria

2.4.7-3.0 Site and Architectural Re-use Criteria

## 1.0 HISTORY

The College Heights Association lemon Packing House provides a link to Claremont's pioneering history in the citrus industry. While largely perceived as a college town, Claremont's major source of income until after the Second World War was the citrus industry. Of the four Packing Houses, only one was devoted to lemons, and its proper operation was of crucial importance to the lemon growers of the town. The Packing House that now stands along First Street is in a deteriorated condition. The Packing House appears much as it did at the height of citrus activity in this area (1920's - 1950's), with some minor exterior and interior alterations. It stands on the north side of the Santa Fe railroad tracks where the lemons were loaded into boxcars for shipment to eastern markets. After the Second World War, the press of land development in Los Angeles County precipitated a decline in citrus industry. In 1972 the packing operations ceased and the building was sold. During the 1980's, portions of the building were leased out for a variety of uses. The east wing was used for bicycle manufacturing; the first floor of the main building housed a variety of retail outlets while the second floor was leased to artists and musicians. In 1991, the entire building was vacated on the order of the fire department due to unsafe conditions.





## 2.0 RE-USE CRITERIA

The intent of the Packing House reuse criteria is to set forth specific guidelines for the preservation and reuse of the Packing House and its site. It is the desire of the City, and of many residents of Claremont, for the Packing House to be renovated and reused. Many suggestions of possible uses have been brought forth; however, at this time no viable use has been determined.

The following criteria apply to the Packing House site within the Village Expansion Area. These criteria apply in addition to the other CMX regulations and guidelines. Typical repair and maintenance activities are exempt from the criteria, so long as these activities result in the same visual character of the structure prior to such work.

In certain cases, these criteria may conflict with the Uniform Building Code (UBC) requirements. In such cases, the Community Development Department can request a review of alternative methods of construction under the State Historic Building Code (Section 104(F) of the UBC). Such requests are made on a case-by-case basis to the City's Building Official. Repairs, alterations, restoration, rehabilitation, or continued use of a building or structure may be made through the identification of an approvable alternative when authorized by the Building Official. The determination of appropriate alternatives to UBC requirements will occur only if any unsafe conditions are reasonably corrected.



*Sawtooth roof forms and rhythm of architectural details are distinguishable features*



*The central section of the Packing House where the lemons were packaged for distribution*



### 3.0 SITE AND ARCHITECTURAL RE-USE CRITERIA

1. Any new use for the Packing House will require modifications to the existing structure, as the building was constructed for an industrial use. Changes to the Packing House building or site should be designed to respect its distinctive exterior materials, features, architectural style, and spatial relationships (zero setbacks, building scale, agrarian forms and projections, windows and doors, and clere-story). Historical features, such as the trusswork and red fire doors in the interior, should be preserved if possible.
2. Work done to meet accessibility requirements and health and safety code requirements, or retrofitting measures to improve energy efficiency, should not change, obscure, damage, or destroy the character-defining materials or features of the building.
3. No increase in height of the structure is permitted unless it can be demonstrated that the visibility of the building projections and defining elements are maintained.
4. Substantial changes to the roofline such as publicly visible roof top appurtenances (including but not limited to solar collectors, decks or mechanical equipment) should not be added unless concealed within the traditional form of the structure. No roofline or parapet extensions should be added to conceal such equipment.
5. New mechanical and service equipment at ground level, such as air conditioning, or transformers, should be installed so that they are inconspicuous from the public right-of-way and do not damage or obscure character-defining features.
6. The mezzanine is a notable element within the structure. If possible, the mezzanine should be maintained with a clear distinction between upper and lower floors.
7. New materials should be similar in texture and appearance to those of the original structure.
8. In any modifications to the existing structure or new additions that are attached to the existing structure, rooflines and pitches, window types, window spacing, door types, overhangs, entries, and treatments on exterior surfaces should be compatible with the historic characteristics of the property.
9. New seismic retrofit systems, whether hidden or exposed, should respect the visual character and integrity of the historic building as much as practicable.





## 2.4.8 ICE HOUSE OFFICE REUSE CRITERIA

### Topics

2.4.8-1.0 History

2.4.8-2.0 Re-Use Criteria

2.4.8-3.0 Site and Architectural Re-use Criteria

## 1.0 HISTORY

The College Heights Association Lemon Ice House Office provides a link to Claremont's pioneering history in the citrus industry. While largely perceived as a college town, Claremont's major source of income until after the Second World War was the citrus industry. The Ice House Office that now stands along Oberlin Avenue is in a deteriorated condition, but the main elements of the structure remain and could be restored.

The small Mission style office of the Claremont Ice Company at 128 Oberlin Avenue was originally built as an Edison sub-station at the corner of First Street and Indian Hill Boulevard. It appears in a photo from the mid 1920's, but there is no date of construction in official records. It was moved to its present site in 1945 to serve as the office of the Union Ice Company. Shortly thereafter, its brick exterior was stuccoed and painted the same cream color as the other Ice Company buildings. The group of buildings were a branch of the Union Ice Company, a national company which served railroad cars. Claremont was plant #15 in the chain and its buildings not only served the railroad but pre-cooled the citrus fruit in the Packing houses of the citrus cooperatives. As the Union Ice Company moved away from serving the railroad, this office became the location of the Claremont Ice Company, which made ice and sold it to local vendors, including the Los Angeles County Fairgrounds.

The Ice House Office retains a similar appearance that it had at the height of citrus activity in this area (1920's - 1950's), with some minor exterior and interior alterations. After the Second World War, the press of land development in Los Angeles County precipitated a decline in citrus industry. In 1972 the packing operations ceased and the collection of ice house and packing house buildings were sold. During the 1980's, the building was leased out office and storage uses. Its unique architecture and connections with the railroad, ice company and citrus industry warrants its preservation in the Claremont Village.





## 2.0 RE-USE CRITERIA

The intent of the Ice House Office reuse criteria is to set forth specific guidelines for the preservation and reuse of the Ice House Office and its site. It is the desire of the City and many residents of Claremont for the Ice House Office to be renovated and reused.

The following criteria apply to the Ice House Office site within the Village Expansion Area. These criteria apply in addition to the other CMX regulations and guidelines. Typical repair and maintenance activities are exempt from the criteria so long as the repair and maintenance results in same visual character of the structure prior to the repair or maintenance.



*Historic Ice House Office*

In certain cases, these criteria may conflict with the Uniform Building Code (UBC) requirements. In such cases, the Community Development Department can request a review of alternative methods of construction under the State Historic Building Code (Section 104(F) of the UBC). Such a request is made on a case-by-case basis of the City's Building Official. Repairs, alterations, restoration, rehabilitation, or continued use of a building or structure may be made through the identification of an approvable alternative when authorized by the Building Official. The determination of appropriate alternatives to UBC requirements will only occur if any unsafe conditions are reasonably corrected.



### 3.0 SITE AND ARCHITECTURAL RE-USE CRITERIA

1. Any new use for the Ice House Office may require modifications to the existing structure, unless the building is to remain as an office use or converted to a museum or similar limited use. Changes to the Ice House Office building or site should be designed to respect its distinctive exterior materials, features, architectural style, and spatial relationships (zero setbacks, building scale, extended parapet and projections, windows and doors). Historical features, such as the original windows and parapet extension should be preserved if possible.
2. Work done to meet accessibility requirements and health and safety code requirements, or retrofitting measures to improve energy efficiency, should not change, obscure, damage, or destroy the character-defining materials or features of the building.
3. No increase in height of the structure is permitted unless it can be demonstrated that the visibility of the building projections and defining elements are maintained.
4. Substantial changes to the roofline such as, publicly visible roof top appurtenances, (including but not limited to solar collectors, decks or mechanical equipment), should not be added unless concealed within the traditional form of the structure. No roofline or parapet extensions should be added to conceal such equipment, unless modifications to the existing parapet won't damage the existing structure in a way that it cannot be permanently replaced.
5. New mechanical and service equipment at ground level, such as air conditioning, or transformers, should be installed so that they are inconspicuous from the public right-of-way and do not damage or obscure character-defining features.
6. New materials should be similar in texture and appearance to those of the original structure.
7. In any modifications to the existing structure or new additions that are attached to the existing structure, rooflines and pitches, window types, window spacing, door types, overhangs, entries, and treatments on exterior surfaces should be compatible with the historic characteristics of the property.
8. New seismic retrofit systems, whether hidden or exposed, should, as much as practicable, respect the visual character and integrity of the historic building.





## 3.0 CIRCULATION

### TOPICS

- 3.1 Existing Roadway Conditions
  - 3.1.1 Existing Street Network
- 3.2 Roadway Improvements
  - 3.2.1 Indian Hill Boulevard and Intersections
  - 3.2.2 First Street – East and West
  - 3.2.3 Bonita Avenue
  - 3.2.4 Oberlin Avenue
  - 3.2.5 Local Streets
  - 3.2.6 Alleys
- 3.3 Pedestrian/Bicycle Circulation
  - 3.3.1 Existing Pedestrian and Bicycle Facilities
  - 3.3.2 Planned Pedestrian Walkways
  - 3.3.3 Planned Pedestrian Crosswalks
  - 3.3.4 Planned Bike Paths/Routes
- 3.4 Public Transit
  - 3.4.1 Bus Transit
  - 3.4.2 Metrolink, Amtrak and Other Transit Providers
- 3.5 Parking
  - 3.5.1 Parking Assessment
  - 3.5.2 Parking Recommendations
  - 3.5.3 Parking Structure-Future Improvement
  - 3.5.4 Parking Management
- 3.6 Streetscape Beautification & Public Improvements
  - 3.6.1 Gateways
  - 3.6.2 Public Pathways, Plazas and Parks
  - 3.6.3 Medians
  - 3.6.4 Street Trees
  - 3.6.5 Lighting
  - 3.6.6 Street Furnishings
  - 3.6.7 Public Right-of-Way Landscaping



## 3.0 CIRCULATION

The Village Expansion Area circulation system represents a logical extension of the village circulation pattern and effectively serves new land uses through an interconnected network of streets and pathways. The circulation system is designed to extend and integrate the existing grid street pattern in a traditional village form. The design character of the street network compliments the existing and planned land use mix and provides a framework for infill development. The circulation design is intended to slow traffic and promote a comfortable pedestrian atmosphere concurrently with maintaining adequate regional commute routes.

The Village Expansion Area circulation plan is comprised of a system of streets, public transit, pedestrian and bicycle routes that connect one location to another, as well as to the surrounding community. Within the system there is a hierarchy of street classifications that include primary streets, secondary streets, and alleys.

In the Village Expansion Area Specific Plan the primary streets provide the main access between the Village Expansion Area and the Village. Primary east/west streets in the planning area include First Street and Bonita Avenue. The primary north/south street is Indian Hill Boulevard. The secondary streets make up the interior system that provide circulation among the land uses.

All streets are to be constructed per the standards of this Specific Plan or its City equivalent. Street design and improvements must address fire and safety access and enforcement, and they should be designed to maximize access by all modes of travel including bicycling, walking, public transit, and driving. Both the Depot and the Metrolink stations are located on First Street in the Village. In an effort to promote public transit use, transit stops may be located on Indian Hill Boulevard to provide access to both the Village and Village Expansion Area.

This Specific Plan addresses the existing and future circulation issues and establishes improvement policies for the Village Expansion Area. The Plan components include roadways and intersections, pedestrian connections, bicycle routes, and public transit. This Section is separated into three primary focuses; the first section addresses technical aspects of the right-of-way development (roadway cross-sections, lane striping, and traffic improvements); the second section addresses the alternative forms of transportation and parking. The third section discusses public streetscape amenities, character and guidelines for future improvements.



## 3.1 Existing Roadway Conditions

The Specific Plan area existing conditions include the Indian Hill Boulevard corridor between Foothill Boulevard and Arrow Highway, the Bonita Avenue corridor between Cambridge Avenue and Yale Avenue, and the portions of First and Second Streets within the existing Village and Village Expansion Area. Analysis of these streets and intersections can be referenced in the Village Expansion Area EIR under separate cover.

### 3.1.1 Existing Street Network

**Indian Hill Boulevard** - considered a secondary arterial in the City General Plan Circulation Element, provides north-south circulation from its interchange with Interstate-10 through the Village Expansion Area and north to the residential areas near the San Gabriel Mountain foothills. From I Interstate-10 north, the street section includes four through lanes with a transition to one lane in each direction at the Bonita Avenue intersection in the Village.

**Foothill Boulevard** - which runs along the alignment of Route 66, is a major regional east-west arterial street. The street carries high volumes of commuter traffic from surrounding communities, and is utilized as an alternate route to Interstate-10. The street has two through lanes in each direction through the City of Claremont, and while an important regional travel route, is outside (north) of the Village and Village Expansion Area.

**Bonita Avenue** - also designated a secondary arterial in the City General Plan Circulation Element, provides east-west circulation from the Village Expansion Area to residential areas to the west. East of Indian Hill Boulevard, Bonita Avenue has one through lane in each direction. West of Indian Hill Boulevard, there are two through lanes in each direction.

**Second Street** - also provides access into the existing Village Expansion Area from Indian Hill Boulevard. Second Street currently terminates at Indian Hill Boulevard at a "tee" intersection.

**First Street** - provides east-west access between the Village Expansion Area and the core of the Village. First Street east of Indian Hill Boulevard has two travel lanes in each direction and is divided by a 30-foot wide median through the Village, Indian Hill Boulevard intersection, and Village Expansion Area.

**Arrow Highway** - is a major east-west arterial that runs between Interstate 605 and Rancho Cucamonga. Like Foothill Boulevard, the route serves as an alternate to Interstate-10 and carries a large amount of regional commuter traffic. The road has two through lanes in each direction through the City of Claremont, while an important regional travel route, is outside (south) of the Village and the Village Expansion Area.





**Cambridge Avenue** - is a north-south local collector street within Claremont, running between Harrison Avenue and Arrow Highway to the west of the Village Expansion Area. The street is comprised of single through lanes and is immediately outside but adjacent to the western boundary of the Village Expansion Area.

**Berkeley Avenue** - is a north-south local street located at the northwest corner of the Village Expansion Area. The street primarily provides access to residential neighborhoods north of Bonita Avenue, though a small section of the street extends into the Village Expansion Area south of Bonita. The street is comprised of single through lanes in each direction.

**Yale Avenue** is a main commercial street in the existing Claremont Village. The north-south local street includes single through lanes in each direction with diagonal parking.



## 3.2 Roadway Improvements

The Village Expansion Area roads, their main intersections, and their planned improvements are shown in the Circulation Design at Build-out Plan, Figure 8. The level of service for each of the individual roadways represents the optimum average daily trips that the General Plan and Village Expansion Area Specific Plan EIR is ultimately targeting for each roadway.

The width of new commercial and residential streets is based on the minimum required to provide safe, efficient vehicular access, emergency access, and where necessary, on street parking. The needs of pedestrian and bicycle circulation has equal consideration in the design of streets. In addition, sidewalks at intersection bulb-outs, and mid-block crossings are expanded to reduce street crossing distances, provide opportunities for public street plaza spaces, allow for decorative street paving treatments and demarcate on-street parking.

The following discussion addresses the key roadways in the Village Expansion Area Specific Plan and identifies the planned improvements for each of the roadways.

### 3.2.1 Indian Hill Boulevard and Intersections

#### The Vision

While Indian Hill Boulevard is adequately designed to handle current levels of traffic, implementation of the Village Expansion Area will change the fundamental nature of the street. As new commercial and residential development occurs on the west-side of Indian Hill Boulevard, the street will no longer be on the edge of the Village but run directly through its center. The vision is for Indian Hill Boulevard to become a thriving, commercial retail street, lined with retail shops, a theater and small cafes in a comfortable walking and driving environment. It is hoped that the Indian Hill Boulevard streetscape will provide an inviting atmosphere for people to shop, sit and leisurely stroll, as they do now on Harvard and Yale Avenues, easily crossing from one side of the street to the other.

Such a transformation of Indian Hill Boulevard is consistent with the long-standing City policy to promote the use of Indian Hill Boulevard as an artery to the downtown area, not as a throughway for north-south traffic. In fact, the City's General Plan has, since the early 1970's, discouraged the use of Indian Hill Boulevard for north-south circulation and encouraged the use of the City's peripheral arterial system comprised of Towne and Monte Vista Avenues.

#### The Challenge

The current five-lane configuration of the street with narrow sidewalks is shown in Figure 9 and is far from being the inviting downtown street the City hopes Indian Hill Boulevard will become. This primary street has an 80-foot right-of-way with four travel lanes, a left turn pocket and right turn trap lanes both north and south bound. The challenge, then, was how best to transform Indian Hill Boulevard into a pedestrian-friendly commercial street, while making sure it can handle the increased traffic volume, resulting from new and infill development.





### The Solution

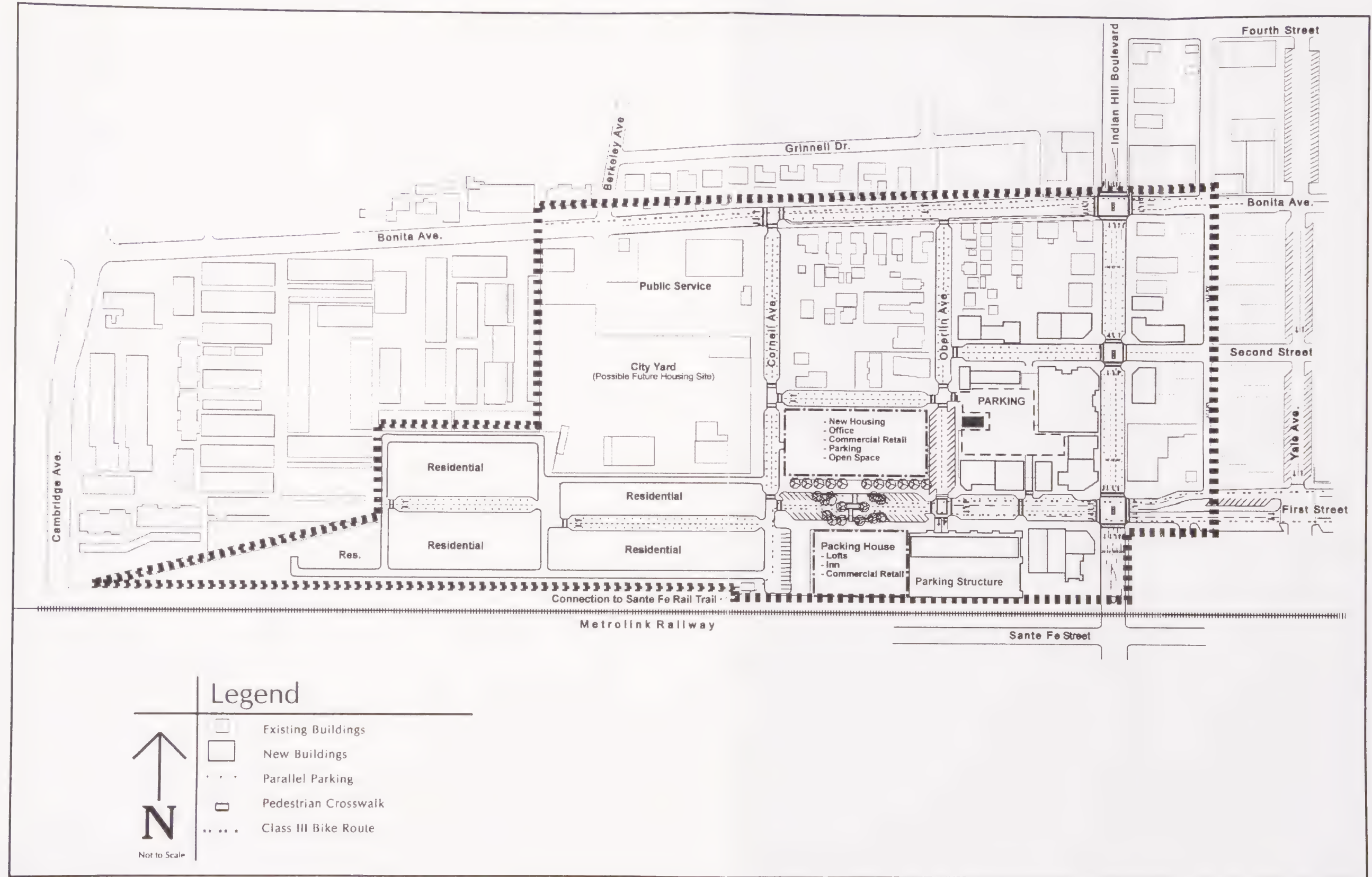
Extensive study and consideration was given to many different options for reducing the large expanse of paving and perceived unsafe conditions for pedestrians along Indian Hill Boulevard. As a result, the planned improvements for Indian Hill Boulevard call for reducing the total number of travel lanes to one north bound and one south bound, with a left turn pocket (See Figure 10). These improvements would promote a slowing of traffic through the Village, allow for a narrower street with wider sidewalks and plentiful planting areas, thus creating the more intimate environment called for in the Village Expansion Area planning principles. Single lanes would still allow Indian Hill Boulevard to adequately handle the anticipated traffic volumes, while also providing for increased vehicular and pedestrian safety.

As shown in Figures 9 and 10, roadway improvements are aimed at reducing the street width and creating a street section that includes two 14-foot travel lanes, a 12' left-turn lane, short right-turn pockets at First Street and Bonita Avenue, an 8-foot wide short-term parking/drop off or bus staging area, and 12-feet of sidewalk on each side. Decorative paving is planned at the three signaled intersections (Bonita Avenue, Second Street and First Streets) to beautify the crossings and provide traffic calming measures. Safe, effective and attractive pedestrian connections across Indian Hill Boulevard will be enhanced and address disabled access and improvements for the visually impaired.

Second Street will be extended (east/west) which will create a new focal point in the Village Expansion Area. Since this east/west artery connects to the heart of the Village, it is planned that Second Street will become an active walking connection between Yale and Oberlin Avenues. As such, the intersection at Indian Hill Boulevard shown in Figure 11 is designed with decorative bulb-outs that will slow traffic and reduce pedestrian crossing distances. As retail and entertainment uses intensify, it is hoped that this intersection evolves into a central activity hub and establishes a strong link between village uses and Village Expansion Area redevelopment. Some of the key elements of the Indian Hill Boulevard streetscape are:

- Signals at First Street, Second Avenue and Bonita Avenue.
  - Synchronized to allow traffic to flow better through the Village.
- Wider sidewalks, narrower street.
  - Sidewalks widened to 12 feet and the street narrowed from 64 feet to 56 feet.
- Bulb-outs at Second Street create a safe pedestrian connection and a strong design statement.
  - Pedestrian crosswalk lengths across Indian Hill Boulevard are significantly reduced.
- Physical streetscape amenities are added to enhance the driving experience and make the street more comfortable to pedestrians.





Striping / Circulation Plan

Figure 8





## The Solution

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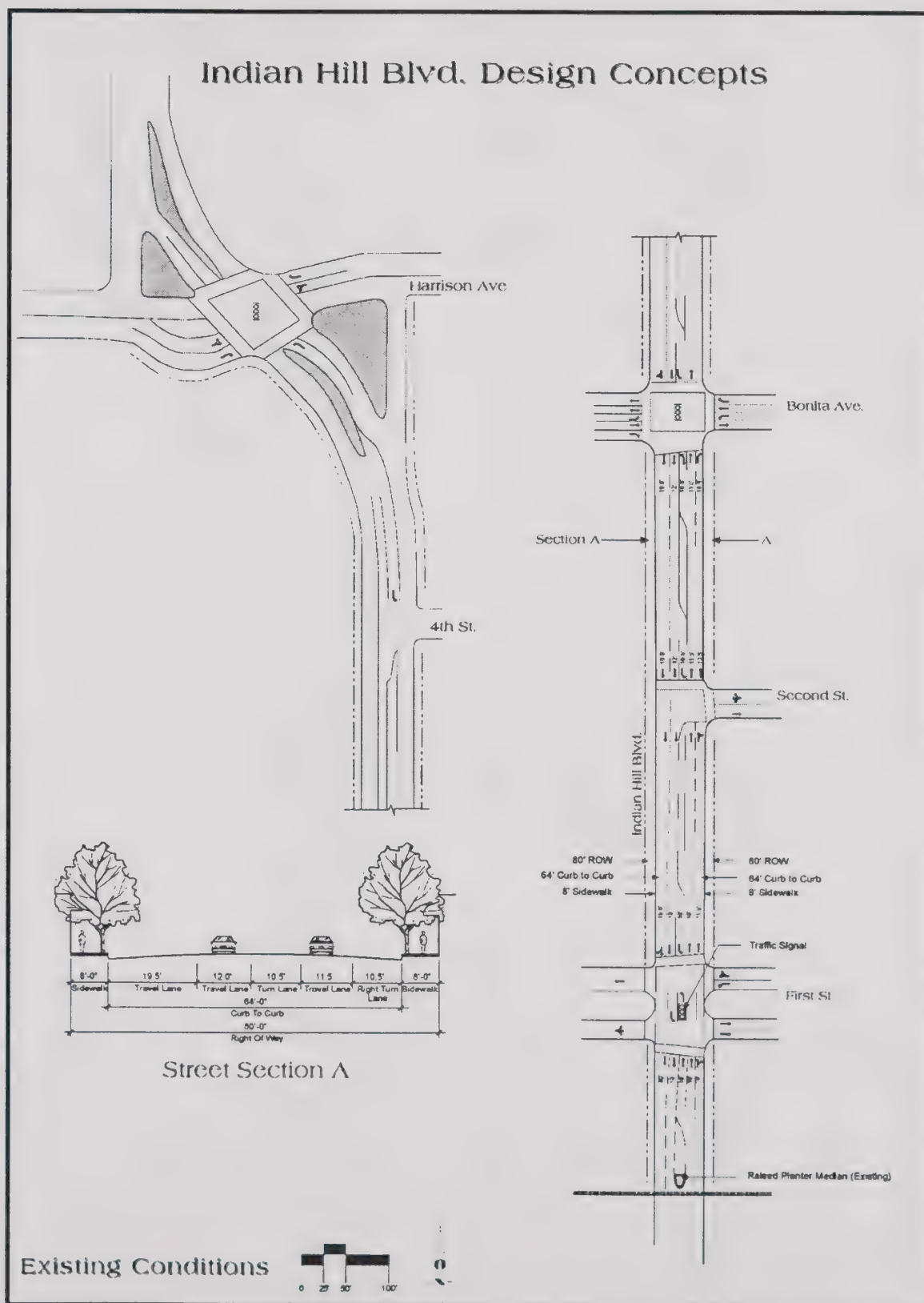
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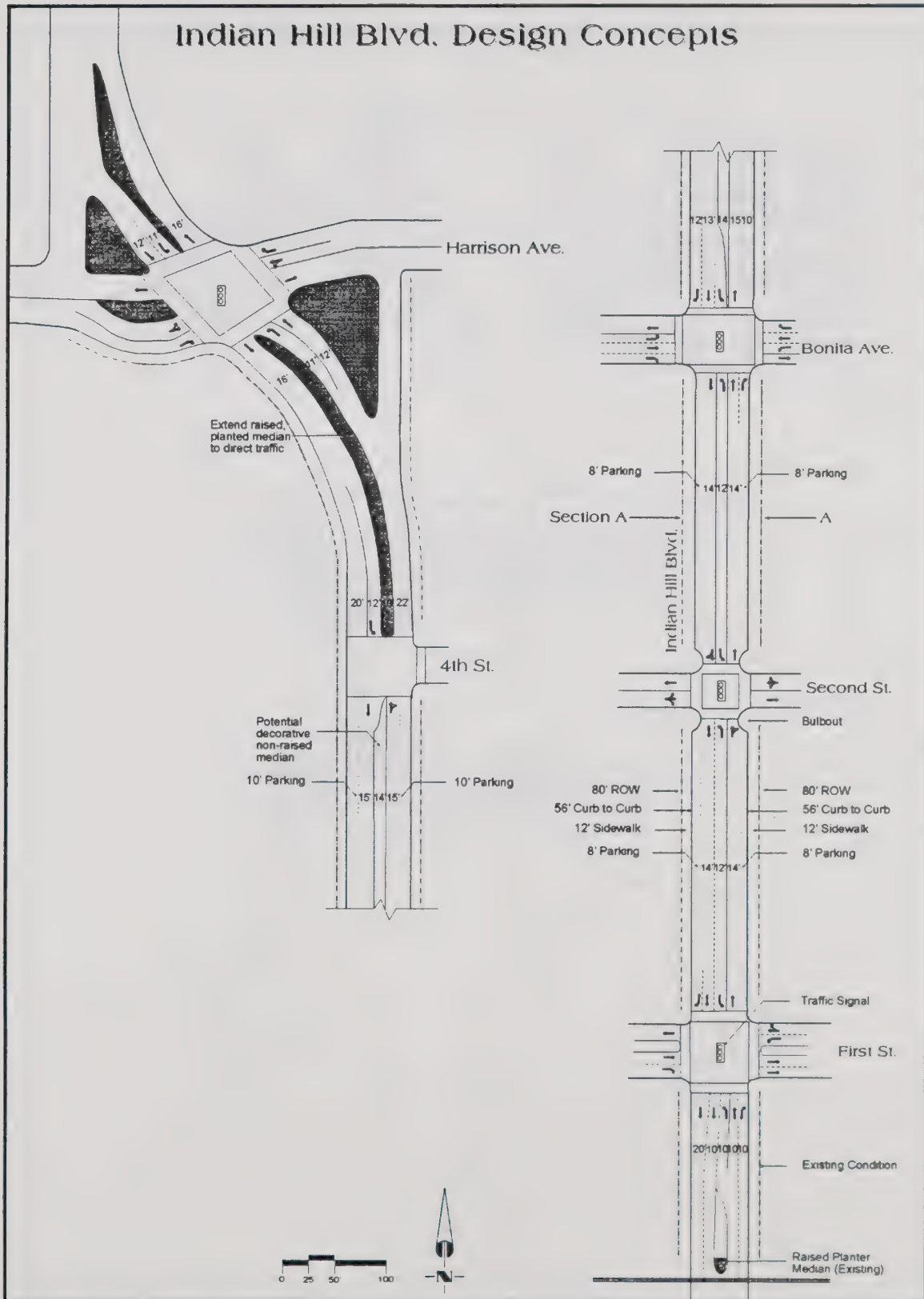




Indian Hill Boulevard Existing Conditions

Figure 9



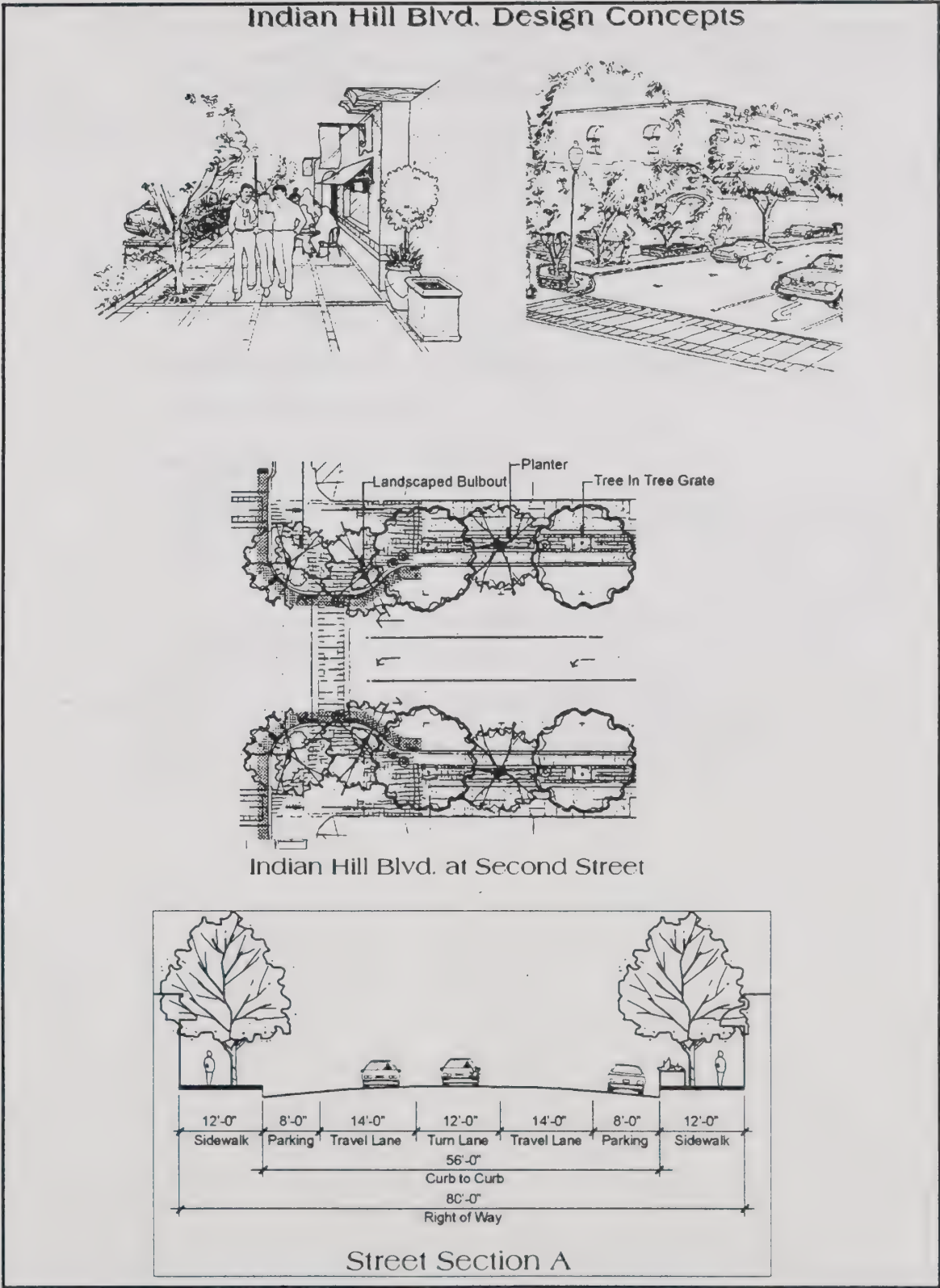


Indian Hill Boulevard Design Concept

Figure 10







Indian Hill Boulevard Design and Streetscape Concepts

Figure 11

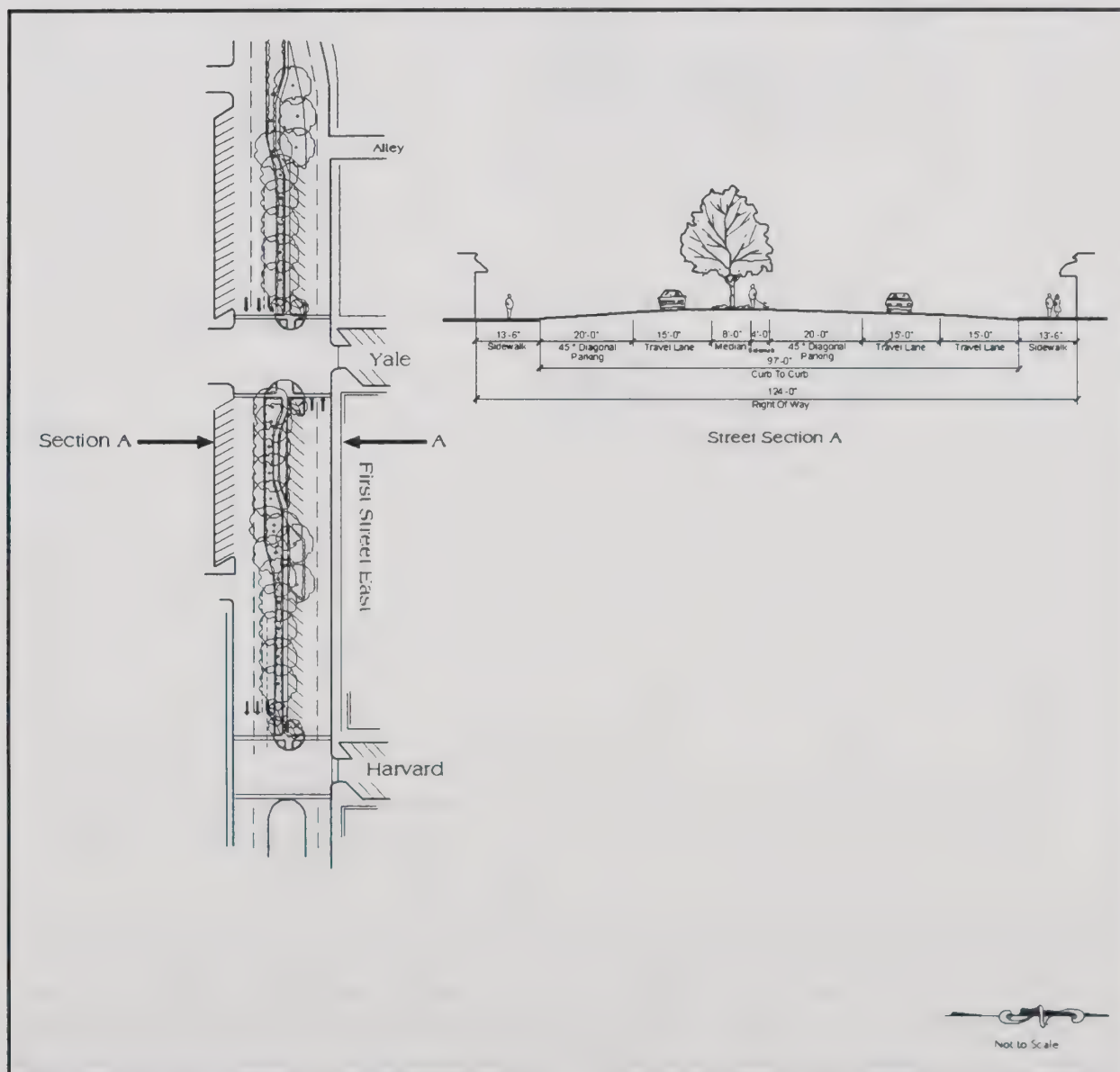


- Wider sidewalks with landscaping and seating areas, buffered from traffic by a parking lane provides an intimate space for pedestrians and also allows for the retention of existing large specimen street trees.
- Wide travel lanes can accommodate bicyclists.
  - Through travel lanes are 14 feet and wide enough to accommodate Class III bike routes.
- Modified lane configurations.
  - One lane of through traffic in each direction.
  - Right-turn pockets at First Street and Bonita Avenue.
  - Left-turn lane throughout.
- Parallel parking on both sides of street.
  - Downtown commercial streets are typically characterized as having fewer travel lanes, narrow pavement widths and parking availability on one or both sides of the street.
  - The parking lane may serve a variety of purposes, including a theater drop-off area and a location for bus stops.
- Raised median extension between Harrison Avenue south toward Fourth Street.
  - Extend raised planted median to direct traffic and announce arrival into the Village.



### 3.2.2 First Street – East and West

A new design for First Street in the Village Expansion Area has been developed in order to transition the pedestrian and vehicular network between the Expansion Area and First Street segment east of Indian Hill Boulevard. The existing median east of Indian Hill Boulevard has been recently reconstructed to add angled parking in the median. This improvement not only created additional parking supply, but reduced the width of the median which caused a barrier between the north and south sides of First Street. This narrower median will allow for a wider pedestrian promenade on the north side of First Street, which will be continued west into the Village Expansion Area, creating a strong pedestrian link across Indian Hill Boulevard.



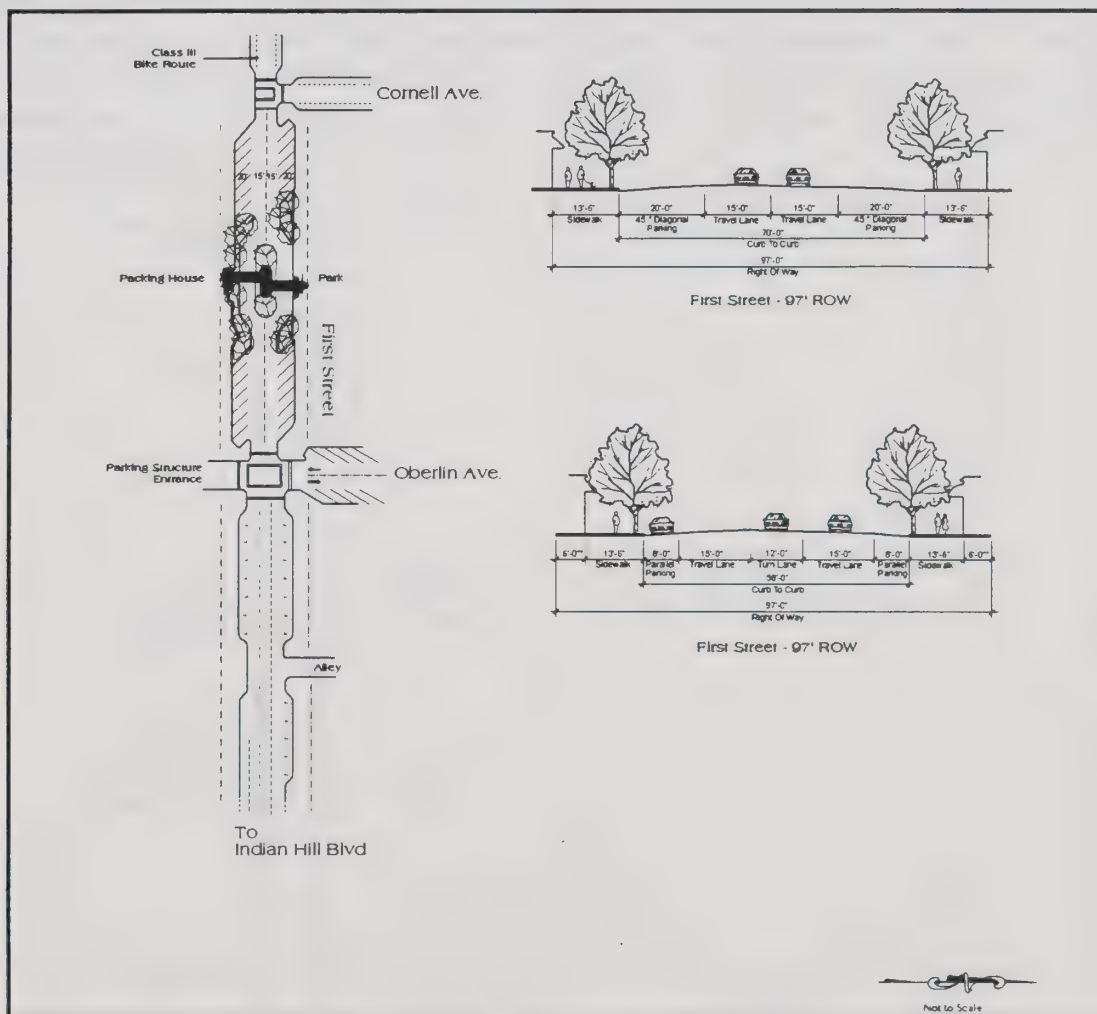
First Street – East Design Concept

Figure 12





The portion of First street that is directly west of Indian Hill Boulevard is shown in Figure 13, and has a 97-foot right of way that is planned for two 15-foot travel lanes, a 12-foot turn lane, 8-foot parallel parking and 13.5-feet of planting and sidewalks on each side. The remaining 12-feet of right-of-way is a key opportunity area and could be used to increase the building frontage/outdoor cafe space, or expanded plaza/sidewalk in order to provide a narrower more comfortable walking environment.



First Street – West Design Concept

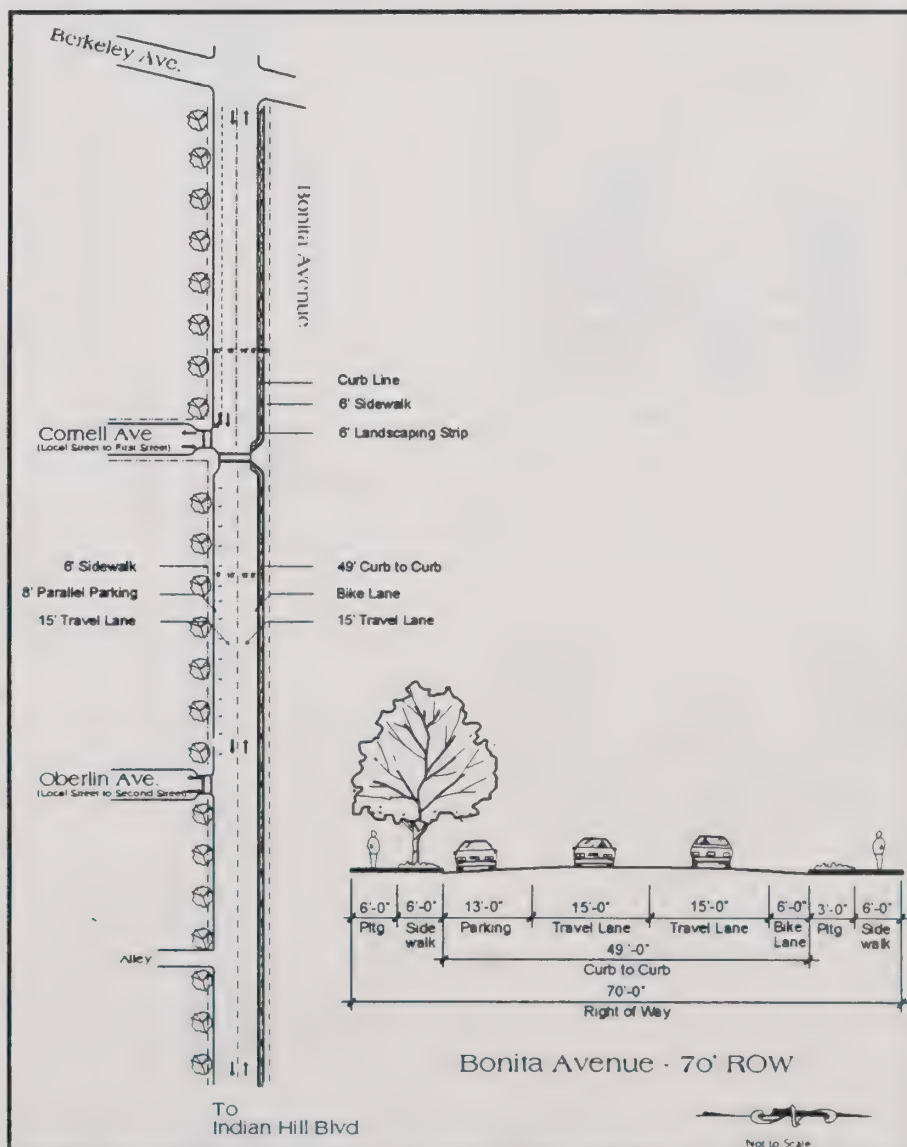
Figure 13

Further to the west, the portion of First Street that is directly west of Oberlin Avenue has a 97-foot right of way. In order to further enhance the pedestrian atmosphere between critical land uses, this public street section is planned for two 15-foot travel lanes, 20-feet of 45 degree angled parking and 13.5-feet of planting and sidewalks on each side. Public right-of-way improvements in this area include an enhanced mid-block pedestrian crossing and plaza which are planned for a later phase of capital improvement that is tied to the future reuse of the Packing House and reuse of the Rich Products site.



### 3.2.3 Bonita Avenue

This main east/west street has a 70-foot right-of-way and the existing vehicle condition contains two 15-foot travel lanes, a center turn lane and 8-foot parallel parking, sidewalk planting, and a continuous sidewalk on the south side of the street. The north side of the street has a non-continuous sidewalk extending from the village westward that breaks at Grinnell Street. As the northern border to the Village Expansion Area, Bonita Avenue has been designed to increase pedestrian safety and access between the Village, Village Expansion Area and the residential neighborhoods to the north and northwest of the Village Expansion Area. This has been achieved by adding a new sidewalk on the north side, while maintaining the existing hedge, of the street and a parkway with street trees on the south side between Indian Hill Boulevard and Berkeley Avenue.



Bonita Avenue Design Concept

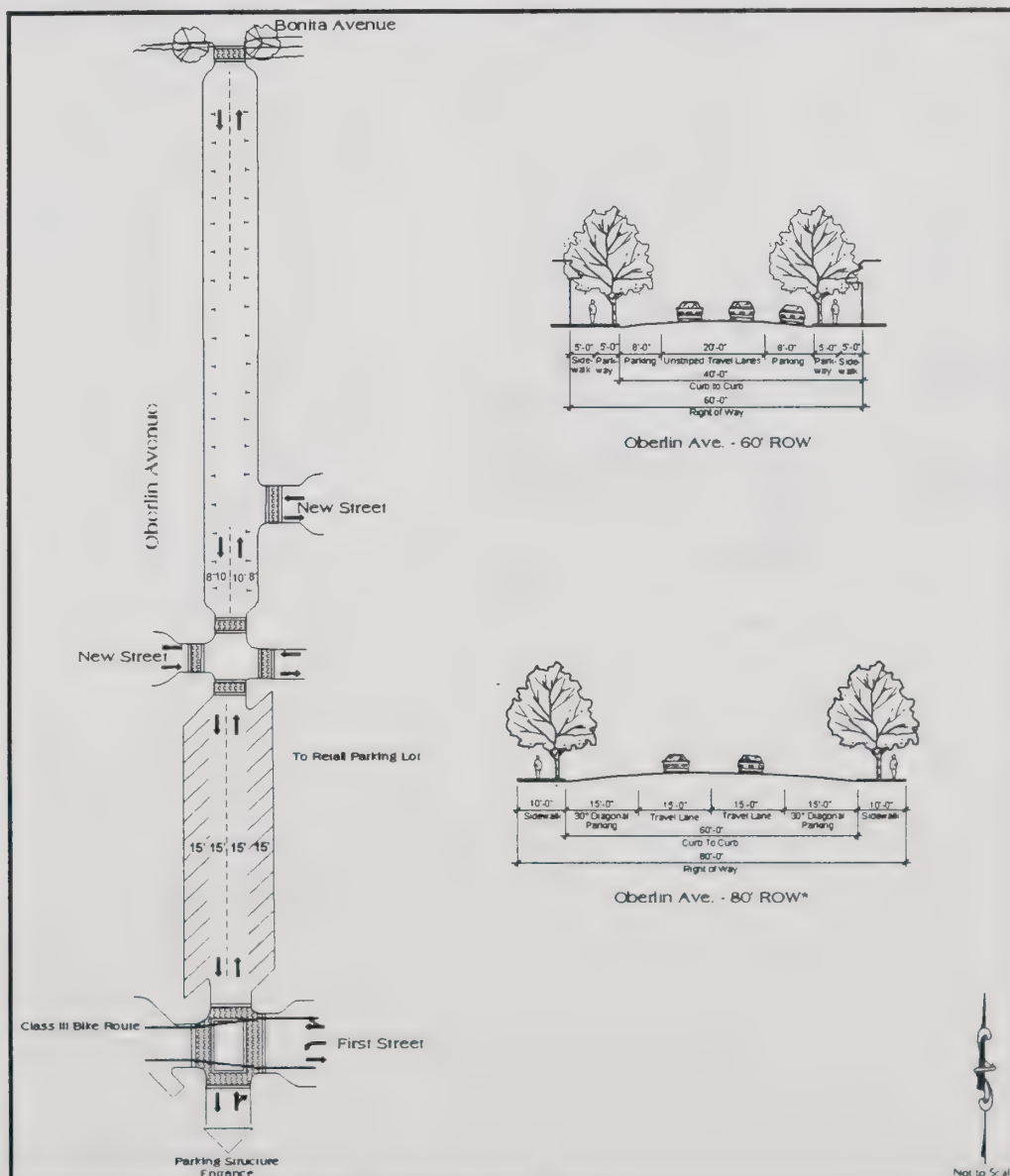
Figure 14





### 3.2.4 Oberlin Avenue

Oberlin Avenue has an 80-foot right-of-way and is planned as a traditional local street in the Village. Second Street has been extended across Indian Hill Boulevard to Oberlin Avenue in order to create an additional link to the existing Village, and reduce the size of blocks consistent with the town's traditional pedestrian grid. In order to mirror the Village pattern in the Expansion Area, the segment of Oberlin Avenue north of First Street is planned to accommodate on-street parking in a pattern that promotes retail and commercial activity. As shown in Figure 15, planned improvements include two 15-foot travel lanes, 15-feet of 30 degree angled parking (on the southernmost portion of Oberlin between the Second Street extension and First Street) and 10-feet of planting and sidewalks on each side of the street.



Oberlin Avenue Design Concept

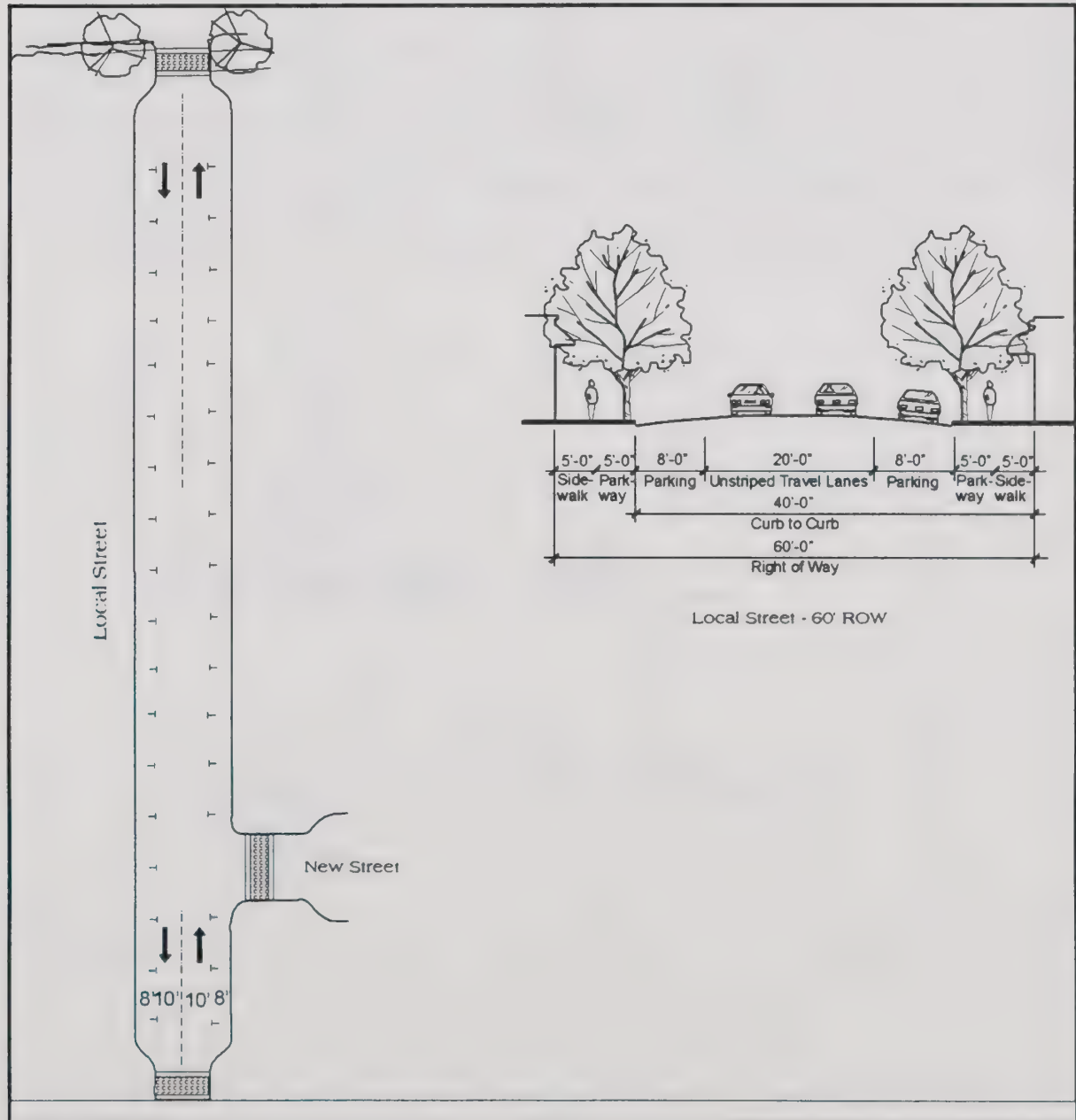
Figure 15



### 3.2.5 Local Streets

A number of local streets are planned in the Village Expansion Area. These streets are intended to evoke a traditional neighborhood design and character that minimizes through traffic, reduces the paved street width and promotes ample sidewalks and pedestrian activity.

A typical local street will have a 60-foot right of way that contains two 10-foot travel lanes that are not striped, 8-foot parallel parking and 10-feet of planting and sidewalks on each side. A narrow parkway that runs between the street and sidewalk is encouraged, wherever possible. All new or improved local streets should be constructed to this standard section.



Local Street (typ.)

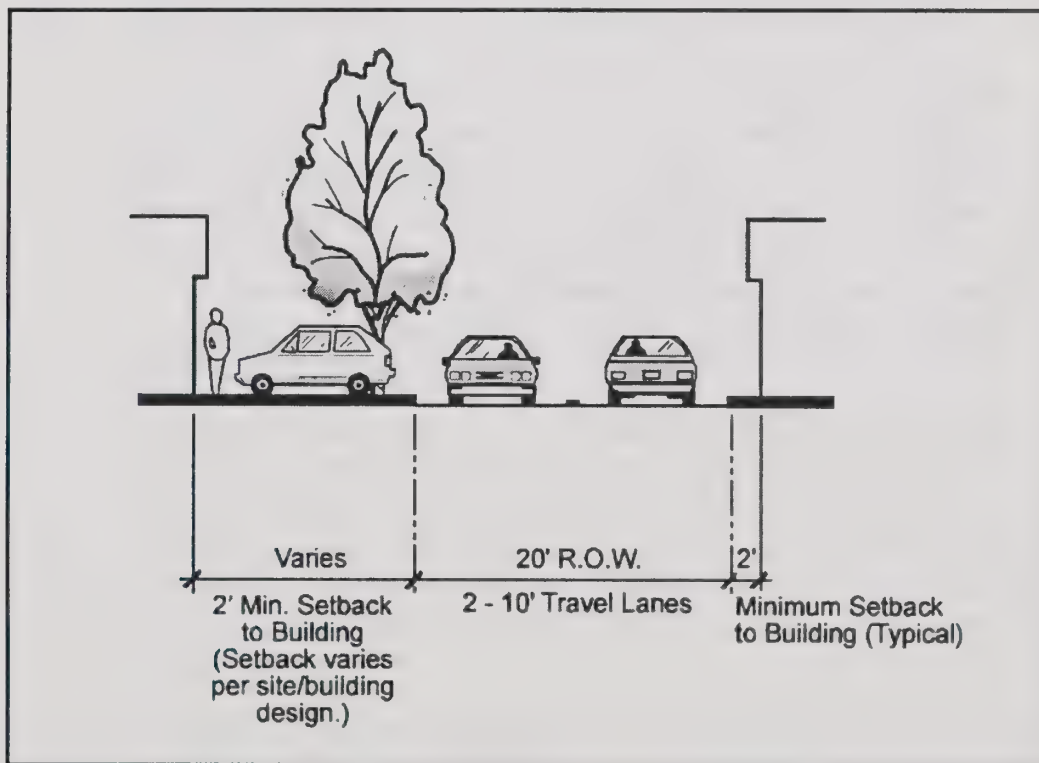
Figure 16



### 3.2.6 Alleys

Alleys are an important element in creating the desired character that blends with the vernacular of the surrounding neighborhood. They have been implemented in order to maintain a pleasant street scene thus allowing for rear entry into buildings, screened service accessibility, and access to mid-block surface parking. Similar to the existing Village, alleys have been designed in a north/south direction, except in the residential area where they function for rear access to lots. In these areas, alleys allow garages to be located behind the house. These lot widths can be narrower and the street scene can be decorated with front porches, stoops and patios versus dominating garage doors.

A typical alley is illustrated in Figure 17 and will consist of two 10-foot travel lanes with a 2-foot minimum setback that will act as a shoulder to provide 24-feet of clear access. This 24-feet of clear access will accommodate service and emergency vehicles. Alley intersections must demonstrate adequate sight distance and provision of appropriate traffic control measures. The precise location of alleys will be determined through the subdivision process. Alleys should be regularly maintained.



Alleyway (typ.) Design Concept

Figure 17





### 3.3 Pedestrian/Bicycle Circulation

The reduction in school bus service, increased congestion and air quality concerns, and regional goals to reduce the number of motorized vehicle trips into the area, have increased the importance of non-motorized transportation in the City's overall transportation system. Non-motorized circulation pertains to the pedestrian and bicyclists. Facilities for these modes of transportation are needed to provide for trips from home to schools, jobs, parks, shopping, and recreation.

#### 3.3.1. Existing Pedestrian and Bicycle Facilities

##### 1. Existing Pedestrian Facilities

Within the existing Village, sidewalks are provided along both sides of all streets. Marked crosswalks are included at each intersection providing access across all streets. Sidewalks on both sides of the street are provided along First Street within the existing Village, both on the outer edges of the travel lanes. Sidewalks cross Indian Hill Boulevard with marked crosswalks and terminate on the west side of the street.

Through the Specific Plan area, the Indian Hill Boulevard corridor includes 8-foot sidewalks along both sides of the street with marked crosswalks at all intersections with the exception of Second Street. Along the two-block segment of Indian Hill Boulevard between First Street and Bonita Avenue, five driveways cross sidewalks on the east-side and seven driveways cross sidewalks on the west side. In addition to the street intersections, these driveways represent potential conflict sites between pedestrians and automobiles. North of Bonita Avenue, multiple driveways access Indian Hill Boulevard as well.

Along the Bonita Avenue corridor, sidewalks and marked crosswalks at intersections are provided within the existing Village. Extending into the Village Expansion Area, the sidewalk on the north side of Bonita Avenue terminates at Grinnell Drive and begins again just east of Berkeley Avenue. No marked crosswalks exist at the intersections of Grinnell Drive, Oberlin Avenue, or Cornell Avenue. At the intersection of Berkeley Avenue, marked crosswalks are provided only on the north and west sides.

Within the Village Expansion Area, sidewalks currently exist only intermittently. Berkeley Avenue includes sidewalks on both sides of the street. A sidewalk exists along the majority of the west-side of Cornell Avenue, but only two short segments exist on the east-side. On Oberlin Avenue, two short segments exist on the west-side of the street, and no sidewalks are provided on the west side. No sidewalk facilities extend into the Village Expansion Area along First Street.



## 2. Existing Bicycle Facilities

Existing bicycle facilities connecting and adjacent to the Village Expansion Area include designated bicycle lanes (Class II) along College Avenue and posted bicycle routes (Class III) along Cambridge Avenue and Eighth Street. The bike route along Cambridge Avenue is proposed to be upgraded to a Class II bike lane according to the City General Plan. Although no designated bicycle facilities currently pass directly through the Village Expansion Area, there are plans for a regional trail and bike route to pass through the area in an east/west direction.

### 3.3.2 Planned Pedestrian Walkways

Pedestrians, whether walking, jogging, or hiking are constantly forced to compete with landscaping, bicycles, and automobiles. In addition, physically handicapped pedestrians have special needs that must be considered in transportation planning. Improvements that promote pedestrian movement should be encouraged and should be ADA compliant. Such improvements include ample public seating, clearly defined crosswalks, identified pedestrian walkways, adequate lighting, fountains, landscaping, and plaza areas. The pedestrian routes should be pleasant as well as safe and practical. Sidewalks and landscaping should be maintained; connections between residential areas, the colleges, the Metrolink and Depot, the business park south of the railroad, nearby churches and retail areas in the Village should be continuous; sidewalk ramps for the handicapped should be constructed and wherever possible, tactile strips or other mechanisms installed to accommodate the visually impaired. Amenities such as benches, lighting, and signage should be provided and appropriately situated to minimize congestion and allow ample room for walking.

Bicycle and pedestrian paths within the Village Expansion Area will compliment the street system and encourage alternative means of transportation while providing safe and convenient connections to destinations within the area and surrounding community. Pedestrian connections to residential neighborhoods adjacent to the Village Expansion Area should be enhanced and are planned along Bonita Avenue to the west and as an extension of First Street to Cambridge. In addition, direct linkages should be provided to transit facilities, and the improvement of First Street both east and west of Indian Hill Boulevard will meet this intent.

### 3.3.3 Planned Pedestrian Crosswalks

Clearly defined pedestrian crosswalks are necessary to provide a continuous network of pedestrian paths and sidewalks. Neck-downs and bulb-outs as well as tactile strips and paving changes in some areas will make pedestrians feel safer and promote walking convenience.

Specific locations will be enhanced to clearly delineate pedestrian crossings, and to create safe walking conditions for all types and ages of users. Expanded sidewalks at intersections and mid-block crossings reduce street crossing distance, provide public street plaza spaces, allow opportunities for decorative street paving treatment and help to screen on-street parking. A





primary area of focus for pedestrian activity is the intersection of Second Street and Indian Hill Boulevard, given it's central location to the existing Village and Village Expansion Area and planned commercial retail and entertainment uses. It is hoped that the extension of Second Street to the west will create a focal point for the Village to transition to the Expansion Area. Plans for intersection bulb-outs will reinforce this intent. Locations planned to receive enhanced pedestrian crossings are shown in Figure 18 and include:

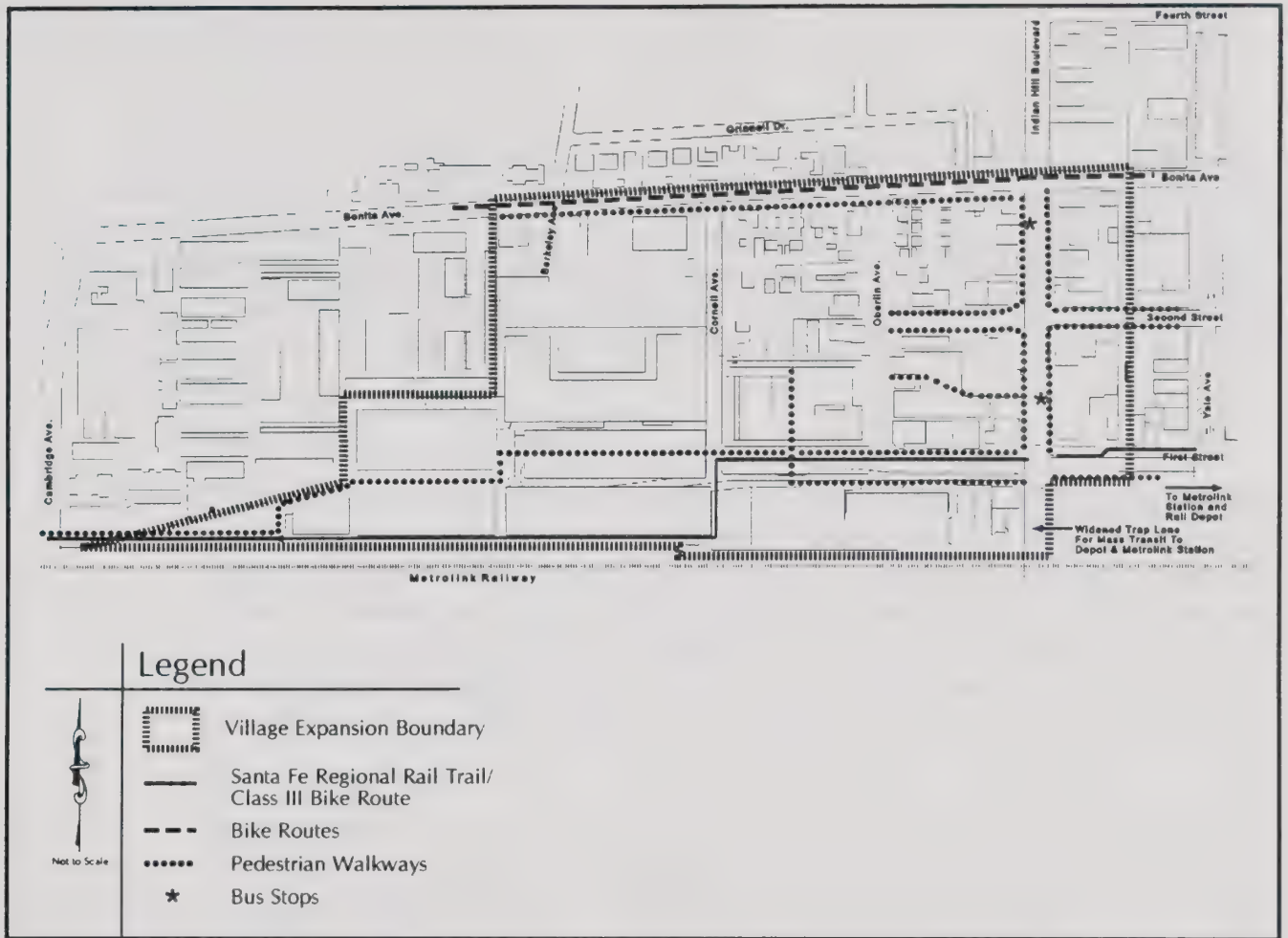
- Bonita Avenue/Indian Hill Boulevard
- Second Street/Indian Hill Boulevard
- First Street/Indian Hill Boulevard
- Fourth Street/Indian Hill Boulevard
- Bonita Avenue/Berkeley Avenue
- First Street/Oberlin Avenue
- First Street/Cornell Avenue

### 3.3.4 Planned Bike Paths/Routes

Much of the Village Expansion Area lies within the City's Bicycle Facilities Plan recognized as consideration for a "potential bicycle priority zone." These are areas where safe bicycle travel is especially important. Specifically, the local regional authority has designated the Citrus Regional Bikeway through the Village and Expansion Area. In order to accommodate this commuter bicycle alignment, a Class III bicycle route (routes provided within a street right-of-way designated by signs or permanent markings) has been provided on both First Street and Bonita Avenue in an east/west direction. The Cities of La Verne, San Dimas and Claremont are currently sponsoring the development of the Citrus Regional Bikeway through funding by the San Gabriel Valley Council of Governments (COG). Through the Village, the rail trail would diverge from the Metrolink right-of-way into the Village Expansion Area. The trail would cross Indian Hill Boulevard at First Street and then follow First Street as a signed bike route until eastern end of the Metrolink parking lot where it would rejoin the rail right-of-way as a separate path. Figure 18 illustrates the planned bike routes within the Village Expansion Area.

Because bicycle travel is highly encouraged for the Village Expansion Area, support facilities are also highly encouraged and recommended to obtain federal funding for bicycle improvement projects. Adequate parking and/or storage at common destinations, bike lockers and racks, resting places and seating areas with shade, etc. should be provided at various locations throughout the Village Expansion Area. Bench/rest stops can consist of a simple bench, small seating area with canopy trees for shade. Bicycle racks can be simply bolted to an existing sidewalk, a plaza, or a paved space in a key area. No specific locations for such improvements have been designated in the Plan, but are encouraged wherever feasible. Merchants can provide these spaces for employees or bike parking/storage can be provided in various plazas throughout the Village Expansion Area.





Pedestrian and Bicycle Circulation Plan

Figure 18



## 3.4 Public Transit

### 3.4.1 Bus Transit

The City currently has a public transportation system that serves the needs of individuals living and working in the community. There are 25 different bus routes in the San Gabriel/Pomona Valley and express service is provided to downtown Los Angeles and Pasadena from Claremont. Service is also provided from the Metrolink station and from local park and ride lots. Greyhound bus service is also provided in Claremont, though the bus stop is located south of Interstate 10 on Indian Hill Boulevard, approximately 1 mile south of the Village Expansion Area.

1. Regional Bus Transit Service

Foothill Transit operates seven public transit routes that pass through the Village. The transit service serves the San Gabriel and Pomona valleys, and links the region to downtown Los Angeles. Fares range from \$0.90 to \$2.75 depending upon the distance traveled and type of bus service (local or express). The Claremont TransCenter is a major stop on all seven bus routes, and is located on First Street just east of the existing Village. Buses also stop at the downtown Metrolink commuter rail station, which is two blocks to the east of the Village Expansion Area.

2. Regional Bus Routes

Route 187 runs between Claremont and Pasadena. Service is provided between 6:00 a.m. and 1:00 a.m. and on weekends, with one stop per hour in each direction during off-peak periods and two buses per hour during peak periods.

Route 195 runs between northern portions of Claremont and the Cal Poly Pomona campus. Service is provided only during a.m. and p.m. peak periods, with four buses running both eastbound and westbound during peak periods. Weekend service is also provided.

Route 292 is a predominantly north-south route that runs between Montclair and Pomona. Service is provided between 6:30 a.m. and 8:30 p.m. and on weekends at the rate of one bus per hour in each direction.

Route 479 runs between Montclair and Cal Poly Pomona. Service occurs hourly in each direction during off-peak times and twice per hour in each direction during peak periods. Buses run between 5:30 a.m. and 8:30 p.m. Weekend service is also provided.

Route 480 provides bus access between Claremont and downtown Los Angeles. The line runs between Montclair and Los Angeles. Service is provided 24 hours per day, seven days a week. One bus per hour in each direction arrives during off-peak times and two buses per hour in each direction during peak periods.





Route 492 also runs between Montclair and Los Angeles, taking a more northern route than route 480. Service is provided twice per hour in each direction between 6:00 a.m. and 10:00 p.m., and on weekends. The route also runs along Bonita Avenue on the northern boundary of the Specific Plan area

Route 690 is an express route between Claremont and Pasadena. Seven westbound buses depart Claremont between 5:00 a.m. and 8:00 a.m., and seven eastbound buses arrive in Claremont between 4:30 p.m. and 7:30 p.m.. Service is not provided on weekends.

### 3.4.2 Metrolink, Amtrak and Other Transit Providers

#### 1. Metrolink Rail Service

The Southern California Regional Rail Authority operates Metrolink commuter rail service throughout the Los Angeles metropolitan region. Claremont is on the San Bernardino line of the system, which links downtown Los Angeles to San Bernardino. Metrolink currently serves Claremont from the Metrolink Station on First Street in the historic depot in the Claremont Village. Twelve round trips are offered between San Bernardino to Los Angeles Union Station with five stations between San Bernardino and Claremont and five stations between Claremont and Los Angeles. Four round trips are scheduled on Saturdays and two round trips on Sundays. Connections are also available to the Red Line Subway in downtown Los Angeles.

#### 2. Amtrak

Amtrak Thruway buses provide connecting service to Amtrak rail lines in Los Angeles. Several Amtrak buses per day stop at the Metrolink Station in the Village.

#### 3. Dial-a-Ride

Service is provided in Claremont by the PVRTA Valley Connection. This is a taxi-based service that transports people anywhere in Claremont for 75 cents. Service is provided on demand.

#### 4. Get About Transportation

Get About Transportation provides door-to-door service for elderly and disabled residents of Claremont, Pomona, La Verne and San Dimas. Service may be used for shopping and social activities as well as business and medical trips. The service currently connects with the San Bernardino County Transit System.



## 3.5 Parking

As build-out continues in the Village Expansion Area, sufficient parking to satisfy the increased demand will be critical. Easy access to off-street parking will eliminate “circling” of the Village streets which impedes existing circulation patterns in the existing Village area. Ample parking will also perpetuate existing and new Village businesses, adding to the overall vitality of the Village and Expansion Area. Off-street parking lots should be created behind proposed and existing buildings whenever possible. Clearly designated signage will direct drivers easily to existing and future parking locations. The following section of the Specific Plan summarizes the parking assessment, existing conditions, peak parking demands and occupancy, planned programs and parking management.

### 3.5.1 Parking Assessment

A parking analysis was prepared by Whitlock and Weinberger, Inc. for the Village Expansion Area that included parking counts conducted in the fall of 1998. (Details regarding this study and its results can be referenced in the Village Expansion Area Specific Plan Final Program Environmental Impact Report and accompanying Appendix). As a result of this study’s findings, policy was set directing that the Village Expansion Area should park itself, thereby placing no pressure on the existing Village businesses to provide parking for new construction and future expansion renovations.

A summary of this analysis is presented below.

#### 1. Existing Parking Conditions

The area included in the parking study conducted for the Village Expansion Area Specific Plan is the area bounded by Fourth Street on the north, First Street on the south, Indian Hill Boulevard plus two public parking lots on the west, and Harvard Avenue on the east. The study area includes 429 public parking spaces, excluding spaces for the disabled, loading zones, and 20-minute zones. (Existing parking supply and occupancy conditions were collected in September, 1998)

The midday peak parking occupancy period occurred during the noon lunch hour between 12:45 and 1:05 p.m. During this time, approximately 85 percent, or 363 of the 429 public spaces, were occupied. This level of parking indicates that the Village Expansion Area will have to rely on parking within the Village Expansion Area rather than utilize parking supply in the existing Village. This confirms City direction that the Village Expansion Area must park itself.

Currently, the Village Expansion Area parking is adequate with single-family unit parking on each individual lot. The commercial ventures in the area do not have high parking requirements due to the industrial nature of each use. However, with denser commercial uses proposed, it is anticipated that this feature may change through the build-out phases of the project.





There are currently 256 parking spaces in the Village Expansion Area. These spaces are comprised of 176 spaces in surface lots and 80 on-street parking spaces. With the estimated 332,639 square feet of existing commercial/retail/office/residential/industrial land uses, the existing parking ratio is one space per 1,299 square feet.

The area included in the Village Expansion Area Specific Plan currently has few marked parking spaces, though ample on-street parking exists. This parking analysis area is bounded on the north by Bonita Avenue, on the south by the Metrolink rail tracks, on the east by Indian Hill Boulevard, and on the west by Berkeley Avenue. The public parking lot at the Metrolink station was not included because of its orientation toward existing Village users and merchants.

## 2. Parking Requirements

The City of Claremont requires a set number of provided parking spaces per unit of development. The total parking demand generated by future development must not exceed the projected parking supply in the Village. The City of Claremont's applicable parking standards for the Village Expansion Area can be referenced in Table 3, Development Standards Matrix and include the following:

- One space per 500 square feet of retail space
- One space per 350 square feet of office space
- One space per 150 square feet of restaurant space
- One space per six theater seats
- One space per 600 s.f. apartment or loft unit
- Two enclosed spaces per detached single family home, .25 visitor spaces per unit
- Two enclosed spaces per attached multi-family unit, .5 visitor spaces per unit
- Residential mixed use units:
  - ≤ 600 s.f. – one enclosed space, .5 visitor spaces (uncovered)
  - ≥ 600 s.f. – two enclosed spaces, .5 visitor spaces (uncovered)

## 3. Peak Parking Demand Occupancy

A detailed analysis of existing parking supply and calculated demand was prepared as a part of this Village Expansion Area planning effort. Specific discussion regarding parking demand can be found in the appendix to this Specific Plan.

Future peak parking occupancy in the combined Village and Village Expansion Area was projected. The observed peak parking occupancy within the existing Village was 85 percent in 1998, during which it was determined that 364 of 429 total parking spaces were occupied. Build-out of the Village Expansion Area, combined with the



added parking in the First Street median, will create a net increase in supply of 732 parking spaces and a net increase in demand of 698 spaces. This translates to an acceptable peak parking occupancy of 91 percent. It is projected that there will be adequate parking capacity upon full build-out of the Village Expansion Area. Parking occupancy should, however, be monitored as development of the Village Expansion Area occurs to ensure that adequate parking supply exists.

### 3.5.2 Parking Recommendations

While it may seem a deterrent to experience parking congestion in the Village, it is a well publicized fact that the perception of a town with a parking problem often means that the area is a success. A Village needs people, and in Los Angeles, people need cars. The simple presence of too many cars can often be a blessing provided vehicle management, signage, circulation and other amenities are properly planned.

As discussed in the Parking Analysis conducted as a part of this Specific Plan effort (See Appendix), there will be adequate parking supply in the Village Expansion Area upon full build-out. Parking occupancy should, however, be monitored as development of the Village Expansion Area occurs to ensure that adequate parking supply exists. In order to properly assure that sufficient parking is met, the City should pursue a number of programs, both as interim and long-term measures.

The City has an established parking in-lieu fee program for the Village. Presently, the City evaluates applications for new construction and decides the applicability of this program on a case-by-case basis. Since Village Expansion Area implementation is planned to occur in phases, the City will continue to evaluate individual development proposals and determine if parking capacity is adequate.

In addition to pursuing the ultimate construction of a parking structure, the City should pursue and implement a shared parking program. The City should monitor the parking demand in the Village and Village Expansion Area under its current program, and develop a pro-active program for attaining adequate parking.

The following parking standards apply to parking within the Village Expansion Area. These parking standards supplement parking standards in the Development Matrix, Table 3 and City Land Use and Development Code.

1. Where a building or buildings located within the Village Expansion Area are non-conforming only by reason of inadequate parking spaces (not including residential conversions to non-residential uses), the provisions of this Specific Plan prohibiting enlargements, structural alteration and expansion shall not apply, provided:
  - That any enlargements, structural alterations or expansions should not further reduce the existing amount of parking spaces, and



- New parking spaces should be supplied to meet the parking requirements for the difference in building area between the existing building and the new building, and
  - New parking spaces should be supplied to meet the difference in parking requirements for the existing building between the prior use and the new use.
2. Whenever possible, the City should limit curb cut entries into properties to maintain sidewalk and streetscape continuity. Shared driveway access on adjacent non-single family properties is encouraged, particularly along Indian Hill Boulevard.
  3. To increase safety in parking lots, separate pedestrian and automobile traffic paths should be designated to minimize conflict areas.
  4. Provide walkways to connect parking lots to building entrances and call attention to internal paseos or walkways. Define walkways by landscaping, lighting, striping and paving.
  5. When surface parking is unavoidable, cluster parking spaces into small parking areas to avoid large paved expanses.

### 3.5.3 Parking Structure – Future Improvement

Under this Plan, a parking structure is an allowable use in the CMX zone. Recognizing the ultimate need for a structure, initial planning has shown a potential location that would be well suited for the structure. The proposed parking structure adjacent to the railroad tracks and the Packing House will assist in meeting the ultimate parking demand for Village Expansion Area build-out. The parking structure will contain multiple levels with commercial/retail uses on the ground level. As development occurs, the City should continue to evaluate parking need in the Village Expansion Area. Periodic assessments should be made as to the need for, and economic wherewithal of the City, to construct a parking structure.





### 3.5.4 Parking Management

As the Village Expansion Area builds out, the actual demand for parking will increase and a parking management plan may be necessary. Small businesses and specialty users that are encouraged in the Village and Expansion Area are typically very sensitive about sufficient parking supply. Therefore, parking management and a strong program to promote sufficient parking should be continued by the City. Since it is apparent that the City's primary focus is to increase the concentration of uses in the Village Expansion Area, as well as encourage a mix of non-competing uses in the Expansion Area, then the effort of parking management and adequate supply is important. Recommended parking management objectives should include re-evaluating current City programs and potentially involve:

1. Re-assessment of the in-lieu fee parking program and its applicability and success in the Village and Expansion Area.
2. Re-evaluation of City parking enforcement, and consideration of increased enforcement of parking restrictions for all parking users to emphasize a consistent and fair enforcement program.
3. Working with the Village and Village Expansion Area merchants to conduct a program so that employees do not park in on-street curb spaces immediately in front of businesses.
4. Explore the use of surface lots (or designated spaces in the parking structure) for a merchant valet program.
5. Improve signage to publicly owned, leased or shared parking lots.
6. Improve pedestrian amenities, access and landscaping of public lots.
7. Pursue constructing a parking structure in the CMX designated properties, with a preferred location on the south side of First Street. This should enable commercial intensities on individual parcels to be increased. This may involve developing an employee shuttle program, combined with reduced or free parking for designated or limited periods to encourage employee use of the structure. Further, pedestrian connections to the structure should be actively pursued with new construction or renovations in the Village Expansion Area.



### 3.6 Streetscape Beautification and Public Improvements

As we continue to design our cities and towns, it is important to understand, recognize, and incorporate by policy true design of good streets. Streets are truly the public's domain, places where everyone goes and where the image of a place is shaped: places where the vitality of a town or an area is measured largely by the composition and life on its streets. Streets provide an intrinsic opportunity to shape and add character to our communities. By embracing the street as an important public place, we create an environment rather than simply a means to get from point A to point B.

In the Village Expansion Area four primary streets play a major role in creating the ambiance and character of the area; First Street, Second Street, Bonita Avenue, and Indian Hill Boulevard. In order to properly establish a long-term vision, revitalization of the Village Expansion Area "streetscape" is necessary to establish a blueprint for renovation and new growth. By painting a picture for these four key Village Expansion Area streets, the City can target a vision for realizing the community's desires for beautifying these important public travelways in the Village Expansion Area. The extension of these central streets into the Village Expansion Area from the existing Village and surrounding residential areas should also be considered for transitional beautification enhancements, and as entry ways into the Village Expansion Area core.

Streetscape beautification is about making streets in the Village Expansion Area comfortable - creating a street scene that is attractive to the eye, un-congested for a social stroll, functional for getting from say the colleges to the Packing House, theater or Depot, and recreational for those wanting leisure. Many elements of the existing Village streets create it's character, from vistas to the San Gabriel Mountains to individual buildings, granite curbs and paving patterns, street furniture to the landscape and storefront activities. This section of the Village Expansion Area Specific Plan addresses those special features, and encourages the City's continued perseverance to maintain a comfortable street.

The Planning Principles Illustration was prepared to guide the City toward a pattern of how the Village Expansion Area could be implemented. It is not intended to be fully implemented immediately. It will take time, money, effort, and partnerships between public and private interests in order to realize its elements. Figure 4 illustrates how the Village Expansion Area may build-out over time. Some of the improvements can be implemented relatively soon; others may take many years to completely realize. Nonetheless, this graphic illustration enables the City and its residents to chart the future path toward renovation of the Village Expansion Area. The guiding design features of the Planning Principles Illustration are:

1. To extend the traditional grid street pattern of the Village in a series of short blocks, that integrate the pedestrian scale, intimate storefronts and mix of residential, commercial and housing with the larger, industrial buildings in the Expansion Area.





2. To create public spaces, plazas, and pathways that serve as community focal points and landmarks and are recognized by the public to position the Village as a cultural center, civic hub, and an identifiably special place for civic events, dining, entertainment, and shopping.
3. To bridge the gap between the Village and Expansion Area by reducing vehicle travel lanes on Indian Hill Boulevard and increasing pedestrian conveniences on the street so that the Village evolves in a cohesive, compact manner.
4. To adaptively reuse culturally significant opportunity sites in a manner that promotes economic viability and retains architecturally rich features.
5. To create physical streetscape improvements that link a pleasant walking environment between the civic center, the Railroad Depot, the Metrolink parking lot, the housing areas on the edges of the Village, and the churches and Colleges that surround the Village and the Expansion Area.
6. To continue the commitment to quality through maintaining compatible buffers and traditional neighborhood design between residential, commercial and public uses.

### 3.6.1 Gateways

To facilitate circulation into the Village Expansion Area and to announce it as a special place within the City, subtle, locally-derived entry monuments that reflect the character of the area should be constructed. Simple, low-level signage should reflect a Village logo and provide a memorable entry to the Village and Expansion Area. The City should consider developing a common Village directional sign that could be accompanied by directional arrows and labeling to denote key shopping points and uses. Signs should be landscaped and placed permanently at the back of the sidewalk or within raised medians at key locations around the Village and Expansion Area periphery.

There are two locations for gateways into the Village. The primary entries into the Village are located on Indian Hill Boulevard. This roadway should contain gateway elements between Harrison Avenue and Bonita Avenue southbound, and northbound between Arrow Highway and First Street. Gateway elements could simply involve the use of distinctive plantings or native materials, or replicating an image or citrus label from the Village Expansion Area's rich citrus industry. Materials and/or elements commonly used in established City medians could provide a unifying feature in a gateway design.

### 3.6.2 Public Pathways, Plazas and Parks

Pedestrian connections should be readily provided through on-street sidewalks, paseos, or alcoves through new buildings to key landmarks such as, the Ice House Office (near the corner of Oberlin Avenue and First Streets), the Packing House, the Jacobsen Building (at the corner





of Bonita Avenue and Indian Hill Boulevard), and the Railroad Depot (east of Indian Hill Boulevard).

#### 1. Public Plazas/Parks

One possible location for a public pocket park or green is on the Rich Products' site. A park site at this location could create a transition between the Village Expansion Area residential neighborhood to the west and the commercial uses to the east. Future build-out of this parcel into a single or combination of uses should include a small park or Village green. This public space could function as a gathering place for public events.

Other locations for small public spaces and parks are suggested in the Planning Principals Illustration, Figure 4.

### 3.6.3 Medians

New raised and planted medians are selectively located on Indian Hill Boulevard north of Bonita Avenue and on First Street west of Indian Hill Boulevard. These medians will control vehicle-turning movements and serve to calm traffic along north Indian Hill Boulevard, and demarcate the pedestrian crossings and walkways along First Street. The medians will also provide a place for directional signage designating internal pocket parking lots and announcing key intersections. The medians will also introduce color and greenery to beautify Indian Hill Boulevard and First Street. The design of the medians, including size and species of plant materials, must be approved by the City Engineer/Department of Public Works. In such cases where raised, planted medians are not functional, decorative paving that defines the space yet allows for vehicle travel is encouraged.

### 3.6.4 Street Trees

The City has developed a street tree planting program with special emphasis on the major traffic arteries of the City; the program is extremely successful and has resulted in creating long linear parks that surround and crisscross through the City. The parkways make an important contribution to the distinctive and attractive appearance of the City; they are an investment in the natural and cultural environment and a benefit to real estate values.

In order to manage and maintain the high quality of street tree planting, the City has developed a Tree Policy Manual that can be obtained from the Community Services Commission. The manual contains tree planting standards, a planned management policy, a construction protection policy for trees to be preserved, and a designated street tree list. New street trees planted in the Village Expansion Area will be based on the designated street tree list as defined and authorized in Municipal Code Section 12.26.010.



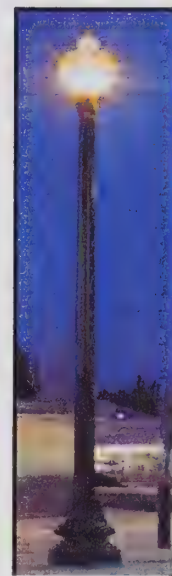
Tree grates are encouraged as they allow for increased sidewalk accessibility and improved maintenance. The installation of tree grates provide room for safe pedestrian sidewalks, improve accessibility, and increase the opportunities for outdoor cafe's and retail kiosks or vendors in public spaces. The use of tree grates is encouraged along heavily used pedestrian streets, such as Indian Hill Boulevard, First Street and Second Street.

If possible, the existing street trees along Indian Hill Boulevard (Jacaranda) should be retained as a part of the City's public improvement program. As the improvement plan calls for the expansion of the existing curb line to widen the sidewalk, the trees should remain in place, and new sidewalk and planters/street furniture and amenities should be placed toward the street edge between the back of the curb and the existing street tree. This design will allow for the retention of mature specimen trees, and new installation of buffer amenities along the pedestrian walkway. Figure 11 illustrates how these existing trees could be incorporated into an improved streetscape beautification plan.

### 3.6.5 Lighting

The City of Claremont has an ongoing installation of decorative theme lights in the Village. These fixtures are beautiful and have significantly enhanced Village character. These fixtures should be continued across Indian Hill Boulevard linking the civic buildings, Village and the Village Expansion Area. The acorn-style globes are mounted onto concrete fluted poles, which could be fitted with bracket hangers to accommodate a custom banner with a selected the Village Expansion Area logo, and to advertise festivities and events, or with or hanging planters.

Along Indian Hill Boulevard, it may be necessary to maintain the cobra-style, high overhead roadway lights due to the user profile, function and nature of the street. The decorative globe lights should be selectively spaced in appropriate intervals in between the cobra lights. Careful design and placement of these poles will be essential to ensure that the addition of poles does not clutter the street or cause congestion for pedestrians. The City may consider a decorative bracket attachment to the cobra poles mounted at pedestrian scale (18' - 20') in lieu of the acorn-style globes.



*Decorative  
Lighting*





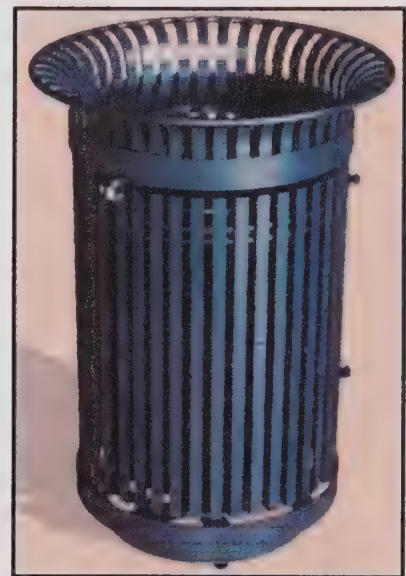
### 3.6.6 Street Furnishings

Streetscape furnishings are elements to be incorporated into the public right-of-way to complement and enhance surrounding development and make the street comfortable for pedestrians. Such elements include street furniture, potted and container plantings, sidewalk and pedestrian amenities, and public art. Streetscape improvements should be implemented as a cooperative effort among the City and private development. Street furnishings and pedestrian improvements planned in the Village Expansion Area include:

A six-foot bench and trash receptacle should be placed every 200 feet along local streets within the CMX designated areas and should be clustered at transit stops and public plazas. As the existing benches and trash receptacles need replacing, they should be replaced with benches similar to the iron benches that now exist and are a part of the City's approved street furniture palette, or an appropriate alternate approved by the City of Claremont Community Services Department. Individual shop or property owners hoping to participate in the City's streetscape beautification program are encouraged to contact the Community Services Department for a list of approved street furniture and an approved landscape palette.



Metal Bench  
Victor Stanley 'Ribbon' Black



Metal Trash Receptacle  
Victor Stanley 'Ribbon' Black





*Precast Concrete Pots*

*Quickcrete 'Tuscon' and 'Provence' Latte*

Bike racks, lockers, newspaper racks, etc., should also be carefully located along the commercial streets – specifically First Street, Second Street, Indian Hill Boulevard, the southern portion of Oberlin Avenue between First Street and the future Second Street/new street extension. The original granite curbs should be retained and recreated wherever possible, particularly at key intersections. Drinking fountains should be located in public gathering areas or plazas. Street furniture should compliment the existing materials and styles currently used in the Village and near the Railroad Depot and Metrolink Station. Other street furniture (public art, banners, telephone boxes, café tables and chairs, moveable container plants and color spots, historical plaques, tables, informational displays, etc.) should be incorporated into streetscape improvements at appropriate locations. Artistic forms and unusual materials that compliment and do not clutter or compete with the existing Village character are encouraged.

The Village Expansion Area Planning Principles Illustration, depicted in Figure 4, addresses a number of beautification elements for areas within the public right-of-way. Some of the envisioned public improvements will require private property owner participation and/or cooperation. Others can be simply implemented by the City as funding is secured. Individual property owner concepts are intended to help guide individual property owners with the future development of their property, but in no way are intended as mandatory plans to be implemented.





### 3.6.7 Public Right-of-Way Landscaping

In addition to street trees, streetscape planting is an inexpensive and easy way to beautify a street. The City should continue its superior quality of installation and maintenance of public landscaping. New plants and trees should match existing or be designed in such a way as to compliment improvements already in place. Planting along the primary streets should consist of planter pots, flower boxes on the buildings, plaza landscaping, and building setback landscaping. Boxed and tub plant containers, such as concrete pots or wooden planters, should be used to enhance sidewalks, plazas and courtyards. Flowering trees (non-fruit bearing) and colorful planting should be used to accent vehicular and pedestrian entries. Vine espaliers and potted plants are excellent elements for screening wall, column and post texture, and should be used in public spaces when appropriate. Particular care should be taken when landscaping active public areas. For instance, planters in parking areas should be designed to account for prevailing direction of pedestrian traffic. In such instances, durable plant species should be selected.



## 4.0 PUBLIC UTILITIES, INFRASTRUCTURE & SERVICES

### TOPICS

- 4.1 Water Service
  - 4.1.1 Existing Conditions
  - 4.1.2 Planned Improvements
  - 4.1.3 Water Service Policies
- 4.2 Sewer Service
  - 4.2.1 Existing Conditions
  - 4.2.2 Planned Improvements
  - 4.2.3 Sewer Service Policies
- 4.3 Stormwater System
  - 4.3.1 Existing Conditions
  - 4.3.2 Planned Improvements
  - 4.3.3 Stormwater System Policies
- 4.4 Solid Waste Disposal
  - 4.4.1 Trash Collection
  - 4.4.2 Recycling
  - 4.4.3 Hazardous Waste
  - 4.4.4 Solid Waste Disposal Policies
- 4.5 Fire, Police Services
  - 4.5.1 Fire Protection
  - 4.5.2 Police Protection
- 4.6 Public Utilities
  - 4.6.1 Public Utilities Policies
- 4.7 Other Public Services
  - 4.7.1 Public Services Policies
- 4.8 Schools



## 4.0 PUBLIC UTILITIES, INFRASTRUCTURE & SERVICES

Public utilities, infrastructure, and service infrastructure are the framework for all well planned communities. Combined with the roadway system, public facilities such as water, sewer, storm drainage, and utilities provide the essential services for the orderly growth of a community. Public services such as fire and police protection need to keep pace with the growth of the community in order to keep it a safe and desirable place to live, work and visit.

### 4.1 Water Service

#### 4.1.1 Existing Conditions

The Southern California Water Company provides water services to the Village Expansion Area. Currently, there are several existing water lines within the planning area that are serving the existing uses. The distribution of these water lines is delineated in Figure 19. These water lines are included in the right-of-way in Indian Hill Boulevard, First Street, Cornell and Oberlin Avenues.

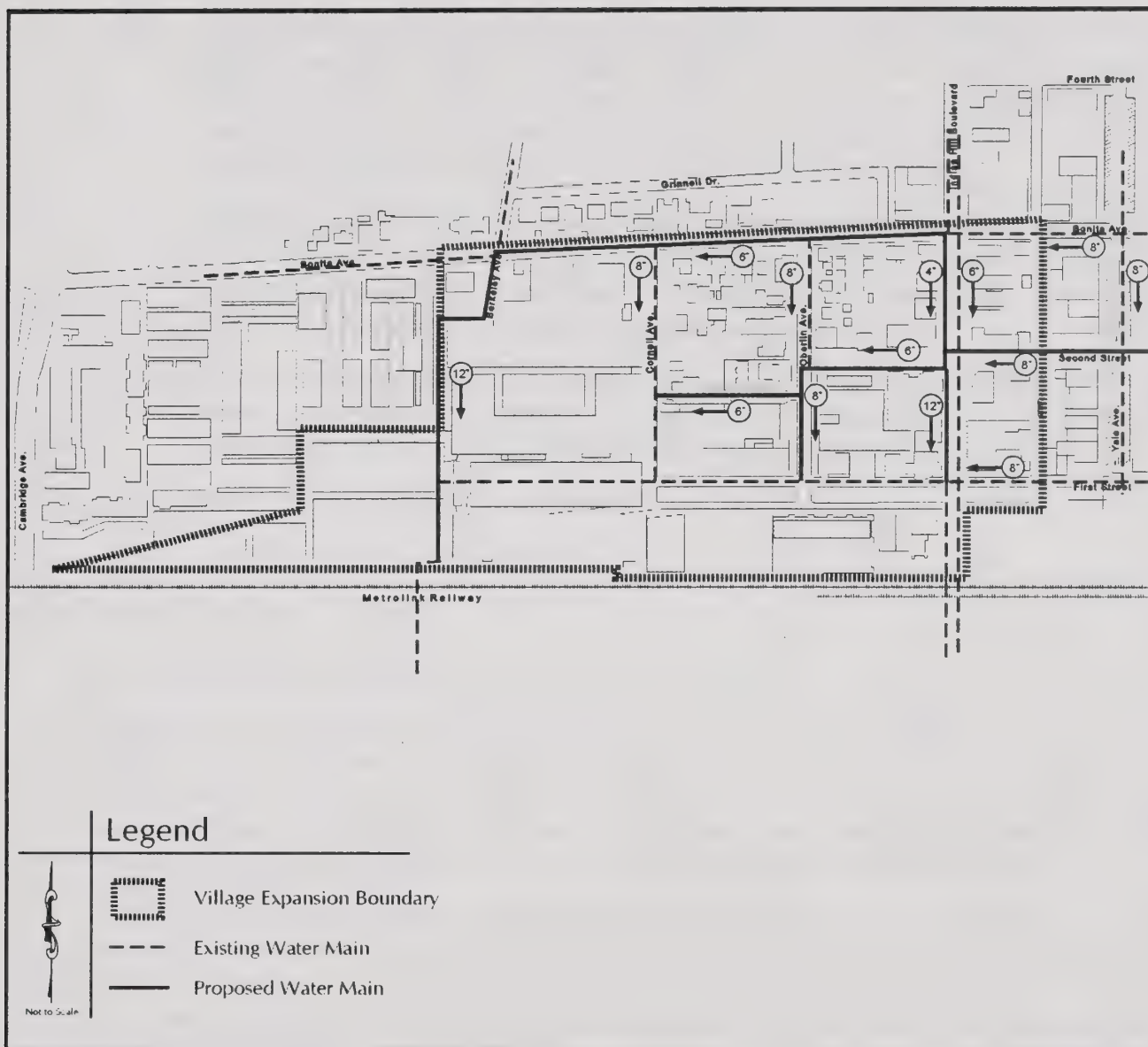
#### 4.1.2 Planned Improvements

While water resources are fully available at a local level to service the project area, water is still considered a limited resource in California. For this reason, it remains important to utilize reasonable water conservation methods.

Additional water service lines are generally routed in the Village Expansion Area to provide service to planned uses. Figure 19 delineates the approximate locations and estimated diameters of these lines. While these plans are subject to further refinement based upon detailed tract map and development planning, the Plan is to provide two 12-inch mains in Indian Hill Boulevard and an extension in Berkeley Avenue south of Bonita Avenue. Additional 8-inch lines are proposed in Bonita Avenue, 2<sup>nd</sup> Street (between Indian Hill Avenue and Yale Avenue), and Oberlin Avenue (between 2<sup>nd</sup> and First Streets). Other system links potentially include a 6-inch line in 2<sup>nd</sup> Street (between Indian Hill Boulevard and Oberlin Avenue) and in the new street between Oberlin Avenue and Cornell Avenue.







Water Distribution System Plan

Figure 19

### 4.1.3 Water Service Policies

The following policies are required of new development in the Village Expansion Area to ensure adequate water supply and service is accommodated.

1. The availability of an adequate water supply must be demonstrated before approving new development.
2. A water distribution system shall be provided which, at a minimum, meets all pressure requirements outlined in the California Department of Health Services/Waterworks Standards.



3. Water supply, storage and adequately-sized pipelines shall be provided to meet fire flows at any point within the Village Expansion Area. Such design shall meet the recommendations of the ISO and/or City Fire Chief and City Engineer, and maintain minimum pressures in accordance with requirements outlined in the California Department of Health Services/Waterworks Standards.
4. An active water conservation program shall be implemented to reduce future water demand to the extent allowed by a law establishing building requirements for new construction, providing educational information through local media sources, and establishing effective rate changes to encourage conservation.
5. New development shall be required to use the best available technologies (BAT) for water conservation, including, but not limited to water-conserving water closets, showerheads, faucets, and water-conserving irrigation systems.
6. Meters for all new water connections shall be required.

## 4.2 Sewer Service

### 4.2.1 Existing Conditions

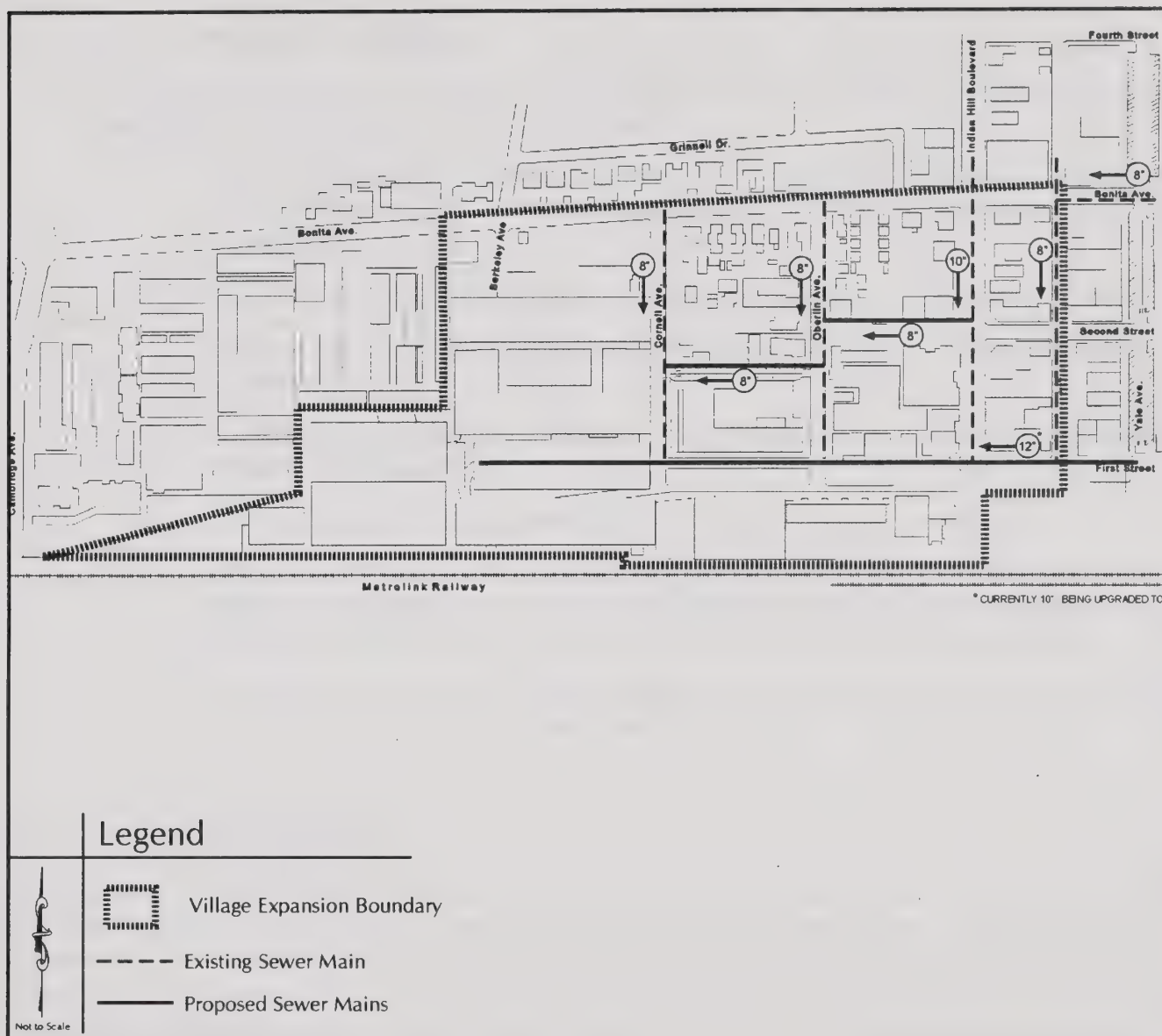
Currently the Specific Plan area site is serviced by the County Sanitation Districts of Los Angeles County (Districts). The Village Expansion Area is located within the boundaries of District No. 21. Figure 20 depicts the existing sewer lines in the planning area. These sewer lines are located in Indian Hill Boulevard, First Street, Cornell and Oberlin Avenues. The direction of flow is south and west toward the southwest corner of the planning area.

The wastewater flow originating from the Village Expansion Area will discharge to a local sewer line, which is not maintained by the Districts. For conveyance to the Districts a 12-inch diameter Claremont Outfall Trunk Sewer, located in the railroad right-of-way at Wharton Drive, was constructed. The trunk sewer had a peak capacity of 4.0 million gallons per day and conveyed a peak flow of 0.6 million gallons per day when measured in 1996 and last measured in 1998. There are no trunk sewers currently within the Village Expansion Area that are maintained by the Districts.

The wastewater generated by the uses in the Village Expansion Area is treated at the Pomona Water Reclamation Plant (PWMP). The PWMP has a design capacity of 13.0 million gallons per day and currently processes an average flow of 11.5 million gallons per day.







Sewer System Plan

Figure 20

### 4.2.2 Planned Improvements

There are two additional sewer lines planned in the Village Expansion Area to complete the existing system linkage. These lines are 8-inches and located in 2<sup>nd</sup> Street between Indian Hill Boulevard and Oberlin Avenue; and in the new street located between Oberlin Avenue and Cornell Avenue. With the addition of these lines, the sewer system for the Expansion Area should be complete and of adequate capacity to serve new development.



### 4.2.3 Sewer Service Policies

The following policies are required of new development in the Village Expansion Area to ensure adequate sewer and service is accommodated.

1. The City shall continue to provide sewerage and operate public facilities. The use of package treatment plants or septic systems to serve individual developments shall not be permitted.
2. Minimum water flows are encouraged through the use of water conservation efforts.
3. Water-intensive uses that use more than 2,500 gallons per day, per acre are discouraged.
4. New development shall be responsible for construction of all sanitary sewer lines serving such development.
5. Cross-connection of sanitary sewer and storm drain systems is prohibited.
6. Collected wastewater shall be of a quality consistent with the State Regional Water Quality Control Board standards or those adopted by the City of Claremont in order to accommodate wastewater within the design parameters of the treatment plant.
7. Connections to the City's existing sewer system and treatment plant shall be approved on a first-come, first-served basis.

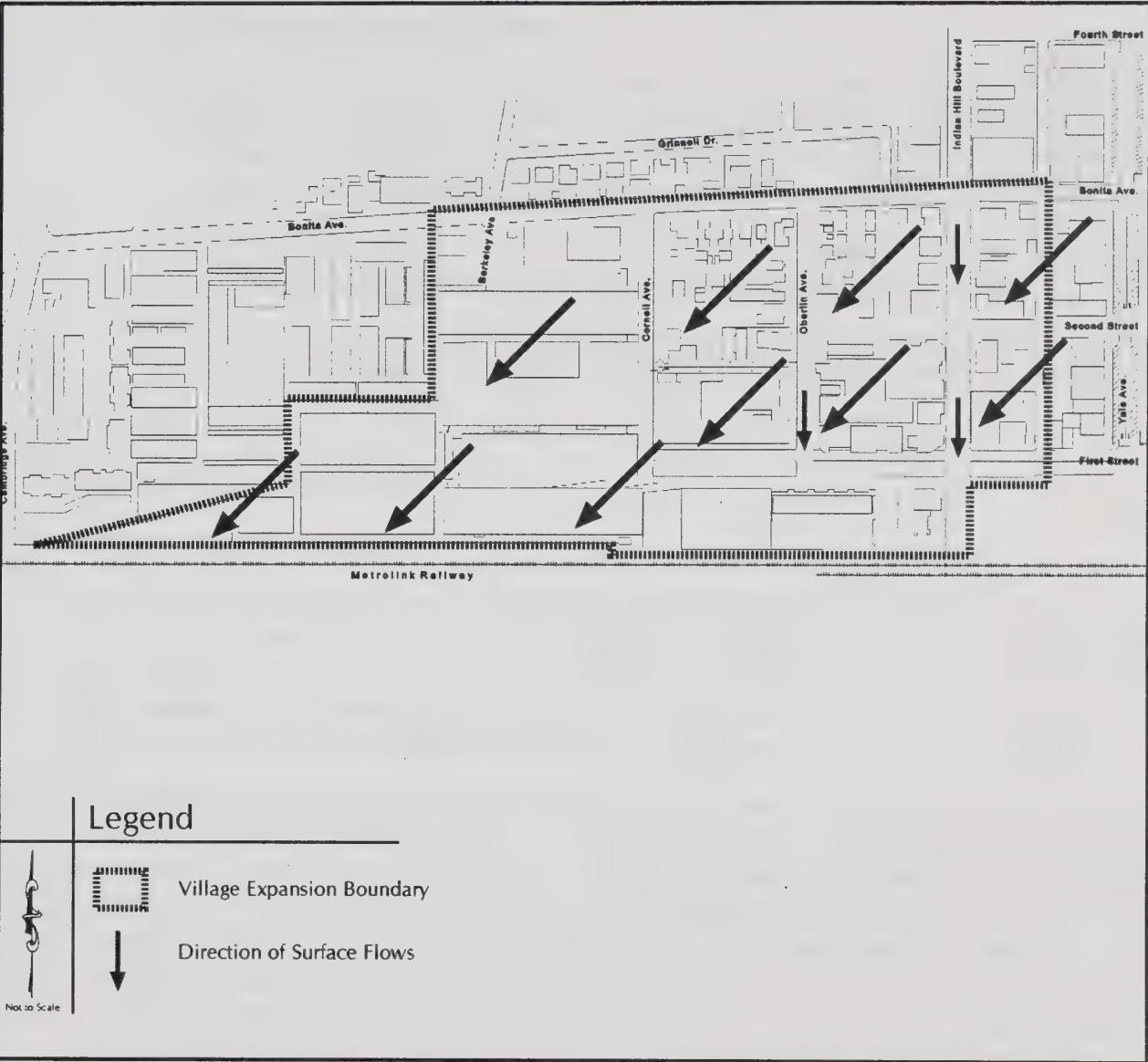
## 4.3 Stormwater System

The following section addresses storm water runoff ~ both quantity and quality. The discussion of stormwater quantity addresses the existing condition and characteristics of the drainage sheds and discusses means of mitigating the potential impacts of the proposed development. The discussion of stormwater quality outlines the factors which affect stormwater quality and the regulatory environment which addresses those factors.

### 4.3.1 Existing Conditions

Currently, surface runoff from the site flows in a southwest direction under the railroad tracks through a detention basin until it is collected by the City's flood control channels which drains into San Jose Creek, then to the San Gabriel River, and ultimately into the Pacific Ocean at Long Beach. In addition, the proposed project drains south to Arrow Highway, then south along Towne Avenue ( See Figure 21) to Drain number 364-6401. Connections to this storm drain require a permit from the County of Los Angeles Department of Public Works. The area does not currently use an underground storm water collection system.





Stormwater Surface Runoff

Figure 21

4.3.2 Planned Improvements

No addition of a stormwater collection system is proposed for the Village Expansion Area. The existing and planned streets will accommodate the anticipated surface flows for the project. Individual project drainage studies will need to be conducted prior to project approvals to ensure that these flows are lower than the maximum standards established by the Agency.





### 4.3.3 Stormwater System Policies

1. Prior to the issuance of grading permits, the City and/or Land Developer shall submit a Stormwater Pollution Prevention Plan (SWPPP) that incorporates Best Management Practices (BMP's) to the State Water Resources Control Board, as required for compliance with the State's General Construction NPDES Permit.
2. Limit the development of impervious surfaces, as much as feasible, within future construction proposals in order to reduce post-project runoff volumes.
3. Require appropriate runoff control measures as part of future development proposals to minimize discharge of urban pollutants (such as oil and grease) into area drains.

## 4.4 Solid Waste Disposal

### 4.4.1 Trash Collection

The City's Community Services Department provides trash collection services to all neighborhoods within the City limits. Residential collection is completed with automated, truck-mounted equipment so City-provided containers must be used. The successful continuation of automated collection of trash and recyclables will depend on the placement of street trees, signs, fire hydrants, and other items, and allowed spots for roadside parking.

The landfill that currently serves the Village Expansion Area is the Spadra Landfill. However, due to the current capacity levels, the landfill is expected to close some time in the 2000 annual year. The landfill that is expected to serve the Village Expansion Area upon the Spadra closure is the Puente Hills Landfill. This existing landfill is located at the juncture of the 605 and 60 freeways in the City of Whittier, approximately 20 miles from the planning area. This landfill is the largest landfill in the United States with approximately 13,000 tons per day. The landfill is owned by the Los Angeles County Sanitation District (SANS District) which is comprised of numerous Cities in the Los Angeles basin and is used by approximately 102 various Cities in the basin. The facility began operations approximately 10 years ago under the current permit which will expire in approximately three to four years. The SANS district is currently applying for another ten year extension on the existing permit. Disposal is expected to continue at the Spadra Landfill until closure.

### 4.4.2 Recycling

The City also offers a curbside recycling program for all residents. This program began in 1983, and has remained in operation using automated containers, commingled recycling, green waste collection since that time. A single blue container is provided to all residents for the collection of aluminum, glass, paper, plastics, and most other recyclable materials.



Claremont generates more than 34,000 tons of solid waste every year and more than half of that amount comes from businesses. The Solid Waste Management Act of 1898 (AB 939) requires that each municipality reduce the amount of waste sent to the landfills 50% by the year 2000. However, the most pressing solid waste issue for the community is the closure of Spadra Landfill. The Puente Hills Landfill is expected to become the replacement for the Spadra Landfill when closure occurs.

#### **4.4.3 Hazardous Waste**

Hazardous waste generated by a typical household (paint, batteries, car oil, pesticides, etc.) requires special handling. Residents are forbidden from disposing tem in regular trash containers due to the fact that they provide a danger to the environment and sanitation workers. Los Angeles County sponsors regularly scheduled, free of charge, “roundups” for the safe disposal of hazardous waste.

#### **4.4.4 Solid Waste Disposal Policies**

1. Comply with the provisions of the City of Claremont’s curbside recycling program.
2. Where possible in new or redeveloped projects, separate recycling container space should be provided in service areas in addition to trash containers.
3. All refuse generated during construction should be separated, and when possible recyclable materials (such as concrete, asphalt, brick, steel) should be diverted from the landfill.

### **4.5 Fire, Police Services**

#### **4.5.1 Fire Protection**

The City of Claremont contracts with the Los Angeles County Fire Protection District to provide fire suppression and paramedic services to Claremont citizens. The fire station, Station 101, is located at 606 W. Bonita Avenue. Current response times for a fire service call to the Village Expansion Area is four minutes or less.

The Village Expansion Area is located in the Very High Severity Fire Zone, as designated by the City’s Building Code. According to the Los Angeles County Fire Department, fire protection serving the area appears to be adequate for the existing development and land uses.

#### **4.5.2 Police Protection**

Police services are currently provided by the City of Claremont Police Department. The Department offers a full range of services including traditional police activities, support of





Neighborhood Watch activities, teaching D.A.R.E. classes, provides disaster preparedness information, and operates volunteer programs. The station is located near the fire station on the corner of Bonita Avenue and Cornell Avenue.

Patrols, and outreach services will continue in the Expansion area as currently being provided. No substations are proposed within the Expansion Area since the present facility is within the planning area and, thus, response times are operating at adequate levels.

## 4.6 Public Utilities

The following utility purveyors provide service to the Claremont Village Expansion Specific Plan Area:

Electrical service	Southern California Edison
Gas Service	Southern California Gas Company
Phone Service	General Telephone Company
Cable TV	Insight Cable Vision

The majority of these services to the downtown area are provided through underground service rather than above ground service poles. The City's Public Works Department will continue to oversee public infrastructure projects which include undergrounding.

### 4.6.1 Public Utilities Policies

1. Promote new communications technologies which will provide future benefit to residents and businesses alike in the form of high speed and high capacity data transfer.
2. Promote high reliability energy sources to secure high technology energy demands.
3. Promote use of solar energy devices and encourage solar orientation of residential and commercial buildings to reduce the use of fossil fuels.

## 4.7 Other Public Services

Public services are an important element to a safe and enjoyable community environment. Maintaining appropriate levels of public service in the area of civic facilities, parks, public safety, schools and libraries, is critical in generating and maintaining a positive image and attracting patrons and tourists to the Village area.



### 4.7.1 Public Services Policies

1. Maintain the provision of adequate public services and facilities to the existing areas of the city and to ensure that new development is served by a full range of public services.
2. Maintain existing public facilities and provide new facilities consistent with community needs.
3. Site new public facilities to avoid loss or destruction of historic or landmark trees.
4. Site and design public facilities to be compatible with adjacent land uses and to enhance the setting of the Village.

## 4.8 Schools

The Planning area is served by the Claremont Unified School District for grades K-12, with approximately 6,300 students within the program. In addition to these public schools for children, the Claremont Unified School District runs an extensive Adult School program. The community also contains the "Claremont Colleges" comprised of seven colleges/universities. Since there are no schools proposed within the Expansion Area, there are three schools within one mile of the project which will serve the residents.

The schools in Claremont that can serve the site include the following:

School Name	Address
Oakmont Elementary	120 Green Street
Sumner Elementary	1770 Sumner Avenue
Sycamore Elementary School	225 West 8 <sup>th</sup> Street
El Roble Intermediate	665 N. Mountain Avenue
Claremont High School	1601 North Indian Hill Boulevard
Mountainview School	851 Santa Clara Avenue

The Claremont Colleges provide a consortium for higher education. The consortium is contiguous, with unique and independent student bodies, faculty, governance and emphasis. The collection of colleges includes five highly regarded undergraduate colleges: Pomona, Scripps, Claremont McKenna, Harvey Mudd, and Pitzer colleges. In addition, there are two specialty colleges including Claremont Graduate University and Keck Institute of Applied Life Sciences.



## 5.0 IMPLEMENTATION, PHASING AND POTENTIAL FUNDING SOURCES

### TOPICS

- 5.1 Introduction
  - 5.1.1 Objectives
- 5.2 Implementation of the Specific Plan
- 5.3 Conditional Use and Parcel map process and review
- 5.4 Phasing
  - 5.4.1 Phasing
- 5.5 Funding and Maintenance of Required Improvements
  - 5.5.1 Financing of Required Improvements
  - 5.5.2 Maintenance of Required Improvements
- 5.6 Timing of Recommended Improvements





## 5.1 Introduction

This chapter is intended to summarize the various plans for Agency and City staff to consider in implementing the Village Expansion Area Specific Plan. These are recommendations only and future decision-makers may choose to implement these ideas in whole or in part.

A commonly raised question is how the City plans to implement the ideas and programs outlined in this Specific Plan. California Government Code Section 65451(a)(4) mandates that a Specific Plan include:

*“A program of implementation measures, including regulations, programs, public works projects, and financing measures necessary to carry out (Chapter 1 – 4) of this Specific Plan.”*

The Village Expansion Area Specific Plan has been prepared pursuant to State law.

Implementation involves securing the necessary capital resources to fund public improvements, agency and developer coordination for areas in the Specific Plan where jurisdictions or properties are affected by specific actions, enforcement of standards and guidelines to ensure new development or renovated structures meet the intent of the Plan, and identifying action steps and programs to fulfill the Plan's many concepts. The following discussion addresses options for these important components.

### 5.1.1 Objectives

The objective of this implementation plan is to generally describe the potential steps to pursue the Specific Plan programs. This section contains the following information:

- Summary of the permitting requirements needed to implement planned development.
- Summary of the planned phasing program.
- A description of recommended funding and maintenance programs.

This Plan sets forth the permits needed to implement development on individual properties within each phase. In addition, this implementation plan identifies the phasing scenarios envisioned and illustrates public improvements that may coincide with future development.

Further, it sets forth a number of recommended funding and maintenance programs that may target revitalization of the Village Expansion Area. These programs are intended to characterize, in a specific way, how the City may want to invest public monies, staff time, and develop private partnerships to fulfill the goals of this Specific Plan.

Thus, this Section (in conjunction with Section 6.0) is intended to set forth the procedural framework that will assure the varied infrastructure needs of the Expansion Area are appropriately orchestrated and the long-term integrity of the Specific Plan goals are met.



## 5.2 Implementation of the Specific Plan

Implementing the Village Expansion Area Specific Plan will require the following actions.

1. Certify the Village Expansion Area Specific Plan Environmental Impact Report (EIR).
2. After certification of the EIR, formally adopt the Specific Plan and any amendments to the General Plan necessary to maintain consistency between the two documents.
3. Prepare and adopt Land Use and Development Code (LUDC) amendments to bring the LUDC in compliance with the Specific Plan, and/or to incorporate the Specific Plan into the LUDC by text and map reference.
4. Implement the Specific Plan through the development review process.
5. In order to allow development of the entire Village Expansion Area Specific Plan, upon adoption of the Specific Plan, the City may, with the request of the individual property owners or other applicants, accept and process conditional use permit and tract/parcel map applications.

## 5.3 Conditional Use and Parcel Map Process and Review

It is the intent of this Specific Plan to facilitate thorough and efficient processing of use permit and parcel/tract map applications filed in conformance with this Specific Plan. Upon adoption of the Village Expansion Area Specific Plan, plans may be processed and acted upon by the City. Plans will be processed according to the development review procedures outlined in Chapter 6 (Development Application Review Procedures) and Article B (Subdivision Ordinance) of the City's Land Use and Development Code.



## 5.4 Phasing

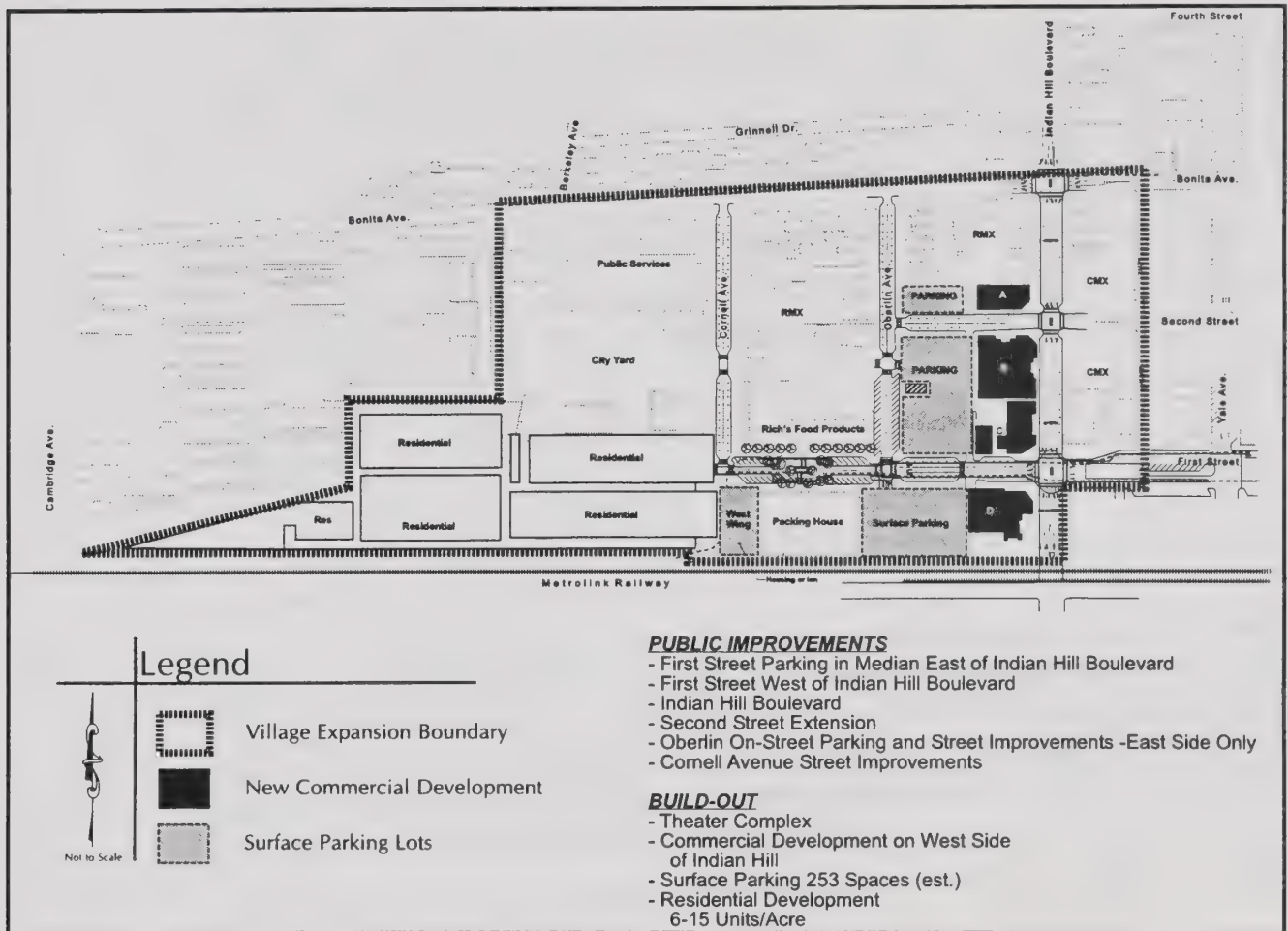
### 5.4.1 Phasing

It is not expected that the Village Expansion Area will be developed all at one time. Rather, individual improvements will occur as market forces and financing mechanisms dictate. Thus, the phasing scenarios shown at this time are intended to provide a general picture of the logical sequence of future build-out in the Expansion Area.

Development within the Village Expansion Area Specific Plan is anticipated to proceed more or less according to the phasing diagrams identified in Figures 22 and 23. This phasing program identifies two primary phases arranged in a manner such that the required circulation, public improvements, and infrastructure improvements necessary to serve proposed developments are installed in each phase, prior to the occupation of dwellings or structures. Development within each primary phase will likely proceed as development applications are made. However, all infrastructure, on- and off-site improvements, and circulation improvements demanded by each development within the phase should be in place prior to occupancy. The details related to these improvements are subject to refinement and change as individual development plans are finalized.







Phasing Diagram - Phase 1

Figure 22

### Phase I Development

The first primary phase will consist of land that is currently shown within Phase I in Figure 22. Figure 22 also identifies the public improvements envisioned in this first development phase. These improvements and related infrastructure, circulation improvements, quantities of land use, domestic water requirements, sewage generation, and other public facilities and services, shall be dictated by the City Engineering Department at the time of individual use permit and tract map applications. The Rich Products facility is a non-conforming use which prohibits their expansion on site. Nevertheless, public streetscape improvements to Cornell Avenue, Oberlin Avenue, and West First Street constructed by the City as part of implementation of the Specific Plan will include interim design solutions that will maintain reasonable, legal truck access to the Rich Products' site, as long as they continue to operate at the present location.



The first phase of development includes commercial uses, residential uses, public improvements, and on-street and surface parking. Specifically, the following uses are envisioned during this phase of development:

- New ground-floor commercial uses along the west side of Indian Hill Boulevard;
- Second floor loft/office space, also along the west side of Indian Hill Boulevard;
- New residential units at a density range of 10 - 15 units per acre on a 6-acre site located west of the center section of the Packing House and south of the City Yard;
- Creation of a "pocket park" at the west end of the project site, adjacent to the proposed regional bike trail located north of the Metrolink right-of-way;
- Buffer improvements between the new housing and the City Yard;
- An "art house" theater on the southwest corner of Second Street and Indian Hill Boulevard;
- Street improvements, including First Street parking in the median east of Indian Hill Boulevard; First Street from Indian Hill Boulevard to Cornell Avenue; Indian Hill Boulevard through the Village; the extension of Second Street from Indian Hill Boulevard and Oberlin Avenue; and improvements to Cornell and Oberlin Avenues, east side only (refer to Section 3.2, Transportation and Circulation, for a detailed description of the proposed street improvements);
- Additional public improvements, including storm drainage, utilities, sidewalks, lighting, and landscaping;
- Parking spaces along the east side of Oberlin Avenue and First Street; and
- Parking spaces in three lots on the east side of Oberlin Avenue and the south side of First Street.



- Second floor loft/office space, also along the east side of Oberlin Avenue and on the northeast and southeast corners of Second Avenue and Indian Hill Boulevard;
- Mixed-uses in the central portion of the Packing House;
- Street improvements along Bonita Avenue, the west side of Oberlin Avenue, and construction of a new street between Oberlin Avenue and Cornell Avenue (refer to Section 3.2, Roadway Improvements, for a detailed description of the proposed street improvements);
- Parking spaces along the west side of Oberlin Avenue;
- Additional public improvements, including storm drainage, utilities, sidewalks, lighting and landscaping;
- Construction of a parking structure.

## 5.5 Funding and Maintenance of Required Improvements

The following section outlines the required public facilities and on- and off-site improvements necessary in order for development to occur within the Specific Plan area. Figures 8, 19 and 20 in the Circulation and Public Services sections of this Specific Plan identify the major public improvements necessary to serve development.

### 5.5.1 Financing of Required Improvements

Financing for the improvements required for development within the Village Expansion Area may be provided by the following sources:

1. Development

Most public facilities and infrastructure improvements required by development in the Specific Plan area will be paid for by development except as expressly negotiated by the Redevelopment Agency in individual DDA's with the participating developer. This is the primary method of funding improvements within the Specific Plan area.

### 5.5.2 Maintenance of Required Improvements

The following section outlines a number of alternative methods that may be utilized to pay for ongoing operations and maintenance expenses for the public improvements required within the planning areas. Most standard public improvements will be owned, maintained, and operated by the City. However, some improvements particular to the Village Expansion Area Specific Plan may require supplemental maintenance funding through alternative methods.





Improvements which are privately owned and benefit residents of a particular project may be maintained by a Homeowners Association.

1. City of Claremont

The City will likely be responsible for operations and maintenance for a majority of public improvements upon completion of subdivision improvements and acceptance by the City. Public streets, water systems, sewer systems, storm drainage, open space, and parks are examples of facilities which would most likely be maintained by the City.

2. Homeowners Associations

Many developments contain improvements that remain in private ownership and are not available for use by the general public. For conditions where this may occur in the Village Expansion Area, a homeowners or business owners association may be established to administer and collect fees for the operation and maintenance of private facilities. The association will elect a board of directors who oversee and administer the association and their duties. If the association defaults on the maintenance of certain improvements, the City may be assigned the responsibility to act in the association's capacity and charge the association for services and/or maintenance provided under their jurisdiction.

## 5.6 Timing of Recommended Improvements

The City Community Development Department in collaboration with the City Engineer shall be responsible for dictating the schedule for constructing required public improvements, both on- and off-site.



## 6.0 PLAN ADMINISTRATION, ADOPTION AND AMENDMENT

### TOPICS

- 6.1 Specific Plan Administration
- 6.2 Specific Plan Adoption
- 6.3 Specific Plan Amendment
  - 6.3.1 Specific Details of Amendment
  - 6.3.2 Presentation of Need for Amendment
  - 6.3.3 Supplemental Environmental Analysis
  - 6.3.4 City Staff Analysis
  - 6.3.5 Public Hearings



## 6.0 PLAN ADMINISTRATION, ADOPTION AND AMENDMENT

This section outlines the measures, actions and policies that are available to implement this Specific Plan. Included are descriptions of administration, regulation, and policy mechanisms that will ensure coordinated development of the Village Expansion Area.

### 6.1 Specific Plan Administration

The Claremont Community Development Department is responsible for the administration, implementation, and enforcement of this Specific Plan. The Community Development Director is responsible for making the determination of whether an amendment to the Specific Plan text or maps is needed. Amendment procedures are described below.

### 6.2 Specific Plan Adoption

The Specific Plan was adopted by City Council Ordinance. Adoption of this Specific Plan involved City Council approval of a General Plan Amendment (text and map). Upon adoption, the Specific Plan, Environmental Impact Report and Mitigation Monitoring Plan established the land use and zoning for the Village Expansion Area. An ordinance amendment was prepared to supplement the City Land Use and Development Code. This ordinance set forth text and map zoning amendments for establishing new zones, specific site development standards and allowable uses for the Village. The City Council adopted this enabling ordinance as a part of the City Land Use and Development Code.

### 6.3 Specific Plan Amendment

Over time, various sections of the Village Expansion Area Specific Plan may need to be revised, as economic conditions or City needs dictate. The policies presented in the Specific Plan contain some degree of flexibility, but any Specific Plan Amendments must be judged by relatively fixed criteria. The California Government Code § 65453, clearly states that a Specific Plan “may be amended as often as deemed necessary by the legislative body.” Amendments to this Plan may be initiated by a developer, any individual property owner or by the City, in accordance with any terms and conditions imposed during the original approval or in accordance with any terms and conditions pertaining to planned development ordinances.

#### 6.3.1 Specific Details of Amendment

Proposals to amend the Specific Plan must be accompanied by detailed information to document the change required. This information should include a revised Specific Plan text (or excerpt therefrom) and revised Land Use Diagram or map amendment, where relevant, depicting the Amendment requested.





### 6.3.2 Presentation of Need for Amendment

Since the City has invested a significant amount of time and money in the preparation of the Village Expansion Area Specific Plan, any proposals to amend the Specific Plan must document the need for such changes. The applicant should indicate the economic, social, or technical issues that generate the need to amend the Specific Plan.

### 6.3.3 Supplemental Environmental Analysis

The applicant must provide an analysis of the Amendment's impacts relative to the adopted Environmental Impact Report. Depending on the nature of the Amendment, supplemental environmental analysis may be necessary. The need for such additional analysis shall be determined by the City of Claremont in accordance with the California Environmental Quality Act (§ 15162).

### 6.3.4 City Staff Analysis

The Director of Community Development shall within ten (10) days of any submittal of a request to amend this Plan, determine whether the amendment is significant or insignificant. If the amendment is determined to be significant, the application shall be reviewed and considered in the manner prescribed by City ordinance. If the amendment is determined to be insignificant, the Director may approve or deny the application. Any decision of the Director may be appealed to the Planning Commission and/or City Council, provided said appeal is initiated within ten (10) working days of receipt by the applicant of written notice of the Director's decision.

*Examples of significant changes include*

- The introduction of a new land use designation not contemplated in this original Specific Plan, or in this Specific Plan as subsequently amended.
- Changes in the designation of land uses affecting two acres or more from that shown in this Specific Plan or in the Specific Plan as subsequently amended.
- Changes to the circulation system or community facility design which would materially affect a planning concept detailed in this Specific Plan or in the Specific Plan as subsequently amended.
- Changes or additions to the design guidelines which materially alter the stated intent of this Specific Plan, or this Specific Plan as subsequently amended.
- Any change which would result in a significant and adverse environmental impact.



### *Necessary Findings*

The consideration of any proposed amendment to this Plan shall include the determination of the following findings:

- Changes have occurred in the community since the approval of the original Specific Plan which warrant approving the proposed amendment.
- The proposed amendment is consistent with the General Plan for the City of Claremont.
- The proposed amendment will result in a benefit to the area within the Specific Plan.
- The proposed amendment will not result in any unmitigated impact to adjacent properties.
- The proposed amendment will enable the deliver of services and public facilities to the population within the area of this Specific Plan.

The City staff will review all of the above-submitted material and provide a staff report for presentation to the Planning Commission and City Council. Staff may also request further clarification of the above studies, if necessary. The staff report will analyze whether the proposed Specific Plan Amendment is consistent with the General Plan and whether the need to amend the Specific Plan can be supported by the conclusions of the supplemental studies.

### **6.3.5 Public Hearings**

If the amendment is considered significant, both the Planning Commission and the City Council must hold Public Hearings on the Specific Plan Amendment, in accordance with § 65453 of the State Government Code.



# Glossary





# A

**Aesthetics.** Characterized by a heightened sensitivity or appreciation of beauty.

**Accessibility.** A means of approaching, entering, exiting, or making use of; passage. The right to approach, enter, exit, or make use of; often used in the form of disabled accessibility.

**Agrarian.** Relating to land, land tenure or the division of landed property.

**Alleys.** A narrow street or passageway between or behind city buildings.

**Amenities.** Something that contributes to physical or material comfort. A feature that increases attractiveness or value, especially of a piece of real estate or a geographic location.

**Arcade.** An arched, roofed building or part of a building. A series of arches supported by columns, piers, or pillars. A roofed passageway or lane, especially one with shops on either side.

**Attached.** Joined to or by a wall, especially by sharing a wall with another building; not freestanding.

**Awning.** A rooflike structure, often made of canvas or plastic, that serves as a shelter, as over a storefront, window, door, or deck.

# B

**Balcony.** A platform that projects from the wall of a building and is surrounded by a railing, balustrade, or parapet.

**Beautification.** The transformation of barren or uninteresting spaces, buildings, forms, structures, into a comfortable or attractive place or environment.

**Bomanite.** A hard, strong construction material consisting of sand, conglomerate gravel, pebbles, broken stone, or slag in a mortar or cement matrix, which includes either integral color or is formed by pressing the concrete material with a decorative pattern.

**Bulb-Out.** A rounded curb or expansion of a sidewalk or crosswalk that reduces the street or paved distance between sidewalks.

**Bungalow.** A single story house where the roof may be made useable with a dormer window. The most familiar style of bungalow is a Western Stick style, characterized by broad, gently pitched roofs, with projecting eaves, constructed in wood with an accentuation of the horizontal.



# C

**Canopy.** A protective rooflike covering, often of canvas, mounted on a frame over a walkway or door or niche.

**Column.** A supporting pillar consisting of a base, a cylindrical shaft, and a capital.

**Complement.** In new construction it means to add to the character of the area by attempting to incorporate similar setback, height, scale, massing, and materials.

**Conversion.** The changing of use or intent.

**Cornice.** The projection at the top of a wall or part of a roof which projects over the side wall.

**Crosswalks.** A path marked off for pedestrians crossing a street.

**Court.** An extent of open ground partially or completely enclosed by walls or buildings; a courtyard. b. A short street, especially a wide alley walled by buildings on three sides. c. A large open section of a building, often with a glass roof or skylight. d. A large building, such as a mansion, standing in a courtyard.

**Curb Cuts.** The elimination of a street curb to enable vehicles to cross sidewalks and enter driveways or parking lots.

# D

**Density.** The number of individuals, such as inhabitants or housing units, per unit of area. The quantity of something per unit measure, especially per unit length, area, or volume.

**Detached.** Standing apart from others; separate or disconnected.

**Detail.** An element of a building such as trim, mouldings, other ornamentation or decorative features.

**Discouraged.** Rehabilitation or new construction methods, design, and/or materials that are not consistent with the overall intent of the Village Expansion Specific Plan. An owner desiring to carry out any “discouraged” item will be required to appear before the Architectural Commission with sufficient justification including drawings for the actions proposed.

**District.** A division of an area, as for administrative purposes. A region or locality marked by a distinguishing feature.

**Dormer Window.** A vertical window which projects from a sloping roof placed in a small gable.



# E

**Early American.** Relating to or characteristic of a style of architecture and decoration produced in the United States especially in the late 18th and early 19th centuries and characterized by adaptations of classical forms combined with typically American motifs.

**Eclectic.** Selecting or employing individual elements from a variety of sources, systems, or styles.

**Edge.** A dividing line; a border, often between zones or land uses, or physical uses.

**Elephant-toe Curb.** Granite and / or concrete raised curb, detailed in a form that resembles an elephant's toe.

**Enhancement.** To make better either functionally or in appearance.

**Espalier.** A trellis of framework on which the trunk and branches of fruit trees or shrubs are trained to grow in one plane.

# F

**Façade.** The entire exterior side of a building; especially the architectural front, sometimes distinguished from the other sides by elaboration of architectural or ornamental details.

**Focal Point.** A building, object, or natural element in a street-scene that stands out and serves as a point of focus, catching and holding the viewer's attention.

# G

**Gable Roof.** A ridge roof that slopes up from only two walls. A gable is the vertical triangular portion of the end of a building from the eaves to the ridge of the roof.

# H

**Hardscape.** Areas which water does not easily penetrate. Surfaces that are not landscaped, i.e., sidewalks, streets, building pads, etc.

**Hedge.** A row of closely planted shrubs or low-growing trees forming a fence or boundary.

**Hipped.** A roof that is sloped on all four sides.

**Historic.** Having importance in or influence on history.

**Homogeneity.** The state or quality of being the same.





# I

# J

# K

# L

**Landmark.** A building or site that has historical significance, especially one that is marked for preservation.

**Loft.** A large, usually unpartitioned floor over a factory, warehouse, or other commercial or industrial space. An open space under a roof; an attic or a garret.

# M

**Maintenance.** The work of keeping something in proper condition; upkeep.

**Mansard.** A hip roof, each face of which has a steeper lower part and a shallower upper part.

**Mixed-Use.** A mixture of different types of land uses in close proximity or walking distance to one another..

**Monolithic.** Exhibiting massive uniformity, singular.

# N

# O

# P

**Parapet.** A retaining wall at the edge of a roof, porch, or terrace.

**Parking.** To put or leave (a vehicle) for a time in a certain location.



**Paseo.** A place that allows for a pedestrian to take a slow, easy stroll or walk outdoors. Often covered or partially covered, the path, series of paths, or walkway along which such a walk is taken.

**Pediment.** The triangular space at the end of a gabled roof, usually low in height compared with the use of its base.

**Pergola.** An arbor formed of horizontal trelliswork supported on columns or posts, over which vines or other plants are trained.

**Pilaster.** A rectangular column with a capital and base, set into a wall as an ornamental motif.

**Pitch.** To set at a specified downward slant: pitched the roof at a steep angle.

**Platted.** A piece of land; a plot. A map showing actual or planned features, such as streets and building lots.

**Plaza.** A public square or similar open area in a town or city.

**Pocket Park.** A very small park that is a sub-component in a larger city. Also called mini-park.

**Porch.** A covered platform, usually having a separate roof, at an entrance to a building. An open or enclosed gallery or room attached to the outside of a building; a verandah.

**Portico.** A structure consisting of a roof supported by columns or piers, usually attached to a building as a porch.

**Preservation.** To keep in perfect or unaltered condition; maintain unchanged. To keep or maintain intact.

## Q

## R

**Recommended.** Appropriate rehabilitation and/or new construction methods, design, or materials for the Village Expansion Area Specific Plan area.

**Reuse.** To use again, especially after salvaging or special treatment or processing.

**Rhythm.** In urban design, the regular recurrence of architectural or natural elements such as even placing of trees down a street, or similar widths and heights of buildings in a street block.

**Ridge.** The horizontal line formed by the juncture of two sloping planes, especially the line formed by the surfaces at the top of a roof.



**Roof Lines.** Various forms to a roof such as pitch, ridge, hip etc, often at different angles.

## S

**Scale.** Harmonious relationship of parts to one another and to the human figure.

**Setback.** In architecture, the recessing of the upper part of the façade due to the smaller area of the upper floors. In urban design, the distance a building is recessed from the curb of the street or from the edge of the sidewalk.

**Sidewalk.** A paved walkway along the side of a street.

**Stoop.** A small porch, platform, or staircase leading to the entrance of a house or building.

**Storefront.** The side of a store or shop facing a street.

**Streetscape.** The overall appearance of a street or grouping of streets in an area.

**Streetwall.** The edges created by buildings and landscaping that enclose the street and create space.

**Stucco.** A durable finish for exterior walls, usually composed of cement, sand, and lime, and applied while wet. A fine plaster for interior wall ornamentation, such as moldings.

## T

**Trash Receptacle.** A fixture for the disposal of garbage. Sometimes ornamental in nature.

**Traffic.** The passage of people, vehicles, or messages along routes of transportation or communication; Vehicles or pedestrians in transit.

**Transit.** Conveyance of people or goods from one place to another, especially on a local public transportation system.

**Trusswork.** Various structural frames based on geometric rigidity of the triangle and composed of straight members subject only to longitudinal compression, tension or both; functions as a beam or cantilever to support bridges, roofs, etc.

## U

## V

**Valance.** A short ornamental piece of drapery placed across the top of a window.





**Victorian.** Being in the highly ornamented, massive style of architecture and decor, popular in 19th-century England. Trends in British architecture and furniture in the Victorian era (1837-1901).

## W

**Western Stick Style.** A style that emphasizes the wood frame and accentuates the horizontal features with broad roofs and deep gables. Shingles are a common wall covering.

## X Y

## Z

**Zoning.** An area or a region distinguished from adjacent parts by a distinctive feature or characteristic. A section of an area or a territory established for a specific purpose, as a section of a city restricted to a particular type of building, enterprise, or activity: a residential zone. See synonyms at area.





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